



Broadway360° Development plan

June 2009

Prepared for: **Broadway Business Improvement District
Nutana Community Association
City of Saskatoon**

Prepared by: **The Planning Partnership**
in association with
Poulos & Chung

Broadway360° Development plan

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acknowledgements

The Broadway 360° Development Plan is a joint initiative between the City of Saskatoon, the Broadway Business Improvement District (BBID) and the Nutana Community Association (NCA). The Plan is the result of a collaborative process that involved a broad representation of people in the community making contributions in a variety of ways.

Numerous residents, businesses, landowners, institutions and organizations have contributed by volunteering their time and providing important information throughout the process. Whether through the Steering Committee, focus group meetings, participation at the Broadway 360° interactive exhibits during the Fringe and Street Fair, or providing input and feedback during the Broadway Summit or Forum, the involvement and support of this community has been invaluable and their participation greatly appreciated.

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The direction, enthusiasm and assistance of the Broadway 360° Steering Committee has been instrumental in the preparation of the Development Plan and we would like to thank and recognize the following members:

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The Planning Partnership

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executive summary

The Broadway area comprises the heart, town centre and main street of the Nutana community. Established as a Temperance Colony in the late 19th century, Nutana is one of Saskatoon's most desirable neighbourhoods, due in large part to its historic small town charm, the success of the Broadway Avenue commercial area, and the proximity to the Downtown and the University of Saskatchewan.

Broadway 360° is a comprehensive development plan for the Broadway Area that will help shape future public and private sector decisions and investments, including guidance on the uses and form of development that is appropriate for this area. In essence, the Broadway 360° Development Plan takes over where the Nutana Local Area Plan left off by adding another layer of detail and guidance for the Broadway commercial district and its relationship to the adjacent residential neighbourhoods.

The Nutana Neighbourhood and Broadway Avenue have a long history of development with numerous citizen and City-led initiatives, each building on the last, with the objective of capturing the character that makes this place unique. The Broadway 360° Development builds on those previous studies and was undertaken through a collaborative 10-month, three-phased process. The process was supported by a Steering Committee comprised of a broad representation of the community, including members of the Broadway Business Improvement District, Nutana Community Association, Broadway Avenue landowners and business owners, residential property owners, City staff and the local Councillor.

The key intended outcomes expressed by the Steering Committee at the outset of the Broadway 360° process was for a Development Plan that included recommendations on the following:

- A clear and distinct definition of the community's 'ideal Broadway'
- Balanced and appropriate land uses on Broadway Avenue and the surrounding area
- Enhancing and/or reinforcing the atmosphere and character of the Broadway area

- Traffic flow management techniques and ways to develop parking capacity to accommodate growth

Summary of Recommendations

The Guiding Vision and Character Areas (pg. 7)

The Five Pillars of the Vision

Informed by the Broadway 360° consultation process and building on the direction in the Nutana Local Area Plan, the following Five Pillars of the Vision give shape and form to what ought to be the ideal Broadway Avenue to work towards.

1. Towards a Sustainable Nutana & Saskatoon
2. Healthy Neighbourhood = Healthy Broadway
3. Leveraging Distinct Character
4. Well Mannered & High Quality New Buildings
5. Pedestrians First

The Ten Big Ideas

The following 10 'Big Ideas' highlight some of the key aspects of the vision and guidance provided in the Development Plan that will shape the future of the Broadway area. They embody the five pillars of the Vision and point the way towards bringing these objectives to fruition.

1. Character Areas & the Neighbourhood
2. An Urban Square with Five Corners
3. A Relocated Community Gardens
4. The Mews – Rethinking the Lanes

5. Two New Potential Linear Parks
6. A Better Integrated Oskayak School
7. The First Three Storeys Matter Most
8. Angular Planes to Ensure Transitions in Height
9. Not More than Nine Storeys & With Conditions
10. Parking Supply is not an Issue – but it can be a Tool

Character Areas

The Broadway area is made up of a number of different parts, each with its own character-defining elements. Collectively, these parts all contribute to the identity of the Broadway area as a whole. These Character Areas inform a finer level of detail for public realm and built form decisions that build on the unique existing conditions, constraints and opportunities.

Broadway Corridor Character Areas

- Broadway Heritage Core
- Broadway South Core
- Broadway Village
- Broadway North Gateway
- Broadway South Gateway

Neighbourhood Character Areas

- Main Street Corridor
- Victoria Corridor
- 8th Street Corridor
- Nutana, Buena Vista and Haultain Neighbourhoods

Public Realm Framework (pg. 27)

The Public Realm Framework defines and guides the Broadway 360° Development Plan with respect to the existing and potential elements that are primarily in public ownership. This includes public uses, open spaces and streetscapes.

Streetscapes

Streets comprise the most significant land area in public ownership and are the primary way in which people experience any given place. More than a road, a streetscape defines and considers all of the elements that combine to shape how a street is experienced, including sidewalks, trees, lighting, furnishing, signage,

and the character and quality of the buildings that define the street wall.

General objectives applicable to all streetscapes include:

- All streets should be lined with trees to enhance visual quality and, given the prairie climate, provide shade and wind breaks.
- Wherever possible, sidewalks should ‘bump-out’ at intersections to define on-street parking areas, lessen pedestrian crossing distances, and to provide ample sidewalk widths.
- Distinguish parallel and angular on-street parking paving to perceptively extend the pedestrian realm and mitigate the width of the roadway.
- Pedestrian convenience and amenities should be of paramount consideration with respect to ensuring adequate sidewalk widths for the desired volume of use and in the siting, design and quality of furnishings, lighting, bus stops and shelters.
- In commercial areas, patios are highly encouraged to further animate the streets in the warmer months.
- Visual clutter and unnecessary obstructions on sidewalks should be avoided and streetscape improvements should continue to integrate lighting, furnishings, newspaper boxes, trash bins, signage within a unified design vocabulary.
- Existing centre boulevards lend to the ‘green’ amenity—they should never be dismantled and when and where possible reintroduced.

The following streetscape types and streetscape elements are addressed with specific recommendations:

- Mixed-Use Streetscapes
- Residential Streetscapes
- Mews
- Lanes
- Crosswalks
- Bicycle Routes

Open Spaces and Connections

Open spaces that are successful can have a tremendous impact on the image, appeal and economic development

of an area. They are also fundamental to the livability of a neighbourhood and to attracting continued residential growth.

Supported by other plans in the Public Realm Framework, open spaces in the Broadway area can be improved and expanded upon through a series of strategies:

- Improve and better utilize existing open spaces by improving visual and physical linkages.
- Identify new and strategic open space opportunities that can serve as catalysts for revitalization and address areas deficient in open spaces.
- Ensure all open spaces are designed so that they will appeal to the broadest demographic including active and passive, soft and hard, formal and informal spaces.
- Enhance visual and physical connectivity to existing and potential open spaces, including the South Saskatchewan River Valley, through streetscapes, pedestrian connections and orienting features such as public art.
- Ensure that open spaces are designed to be safe and in accordance with CPTED principles.
- Ensure private amenity spaces such as roof gardens and encourage publicly accessible spaces within new significant developments.
- Ensuring that streets continue to provide open space and 'green' amenity through high quality streetscaping.

The following open space types are addressed with specific recommendations:

- Parks and Green Open Spaces
- Plazas
- Community Garden & Mid Block Connection
- Creating Safe Public Spaces

Civic Elements

The identification of these elements ensures a coordinated approach to the design of buildings, streets and open spaces so as to reinforce visual connections into and throughout the Broadway area.

The following civic elements are addressed with specific recommendations:

- Gateways
- Public Art Sites

Development Framework and Design Guidelines (pg. 41)

The Development Framework builds on and reinforces the Vision, Character Areas and the Public Realm Framework to provide guidance for new buildings in the Broadway 360° area. The purpose is to develop a coherent, rational and integrated guide for the future built quality and character of the Broadway area as shaped by the use, massing, scale, typology and design of buildings.

Development Principles

A central objective of the Development Framework is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm. Informed by the consultation process and rooted in good planning and urban design practice, the key principles that underpin the Development Framework include:

- Protecting heritage resources and retaining their visual prominence.
- Protecting and strengthening established low-rise residential areas and ensuring compatible infill development and sympathetic developments in adjacent higher density areas.
- Ensuring base building conditions that form an appropriately scaled and designed street wall that reinforces the desired character at the street level.
- Ensuring appropriate building massing and height taking into consideration existing and permitted heights; proportional relationships to streets; and, visual and physical impacts on pedestrians and adjacent areas.
- Ensuring that new developments provide for appropriate transitions between areas of differing intensities and scales.

- Reinforcing important intersections and corners through massing and design.
- Well designed and articulated buildings that positively contribute to the quality and animation of the streetscape and the overall defining character and image of the area.

Components of the Development Framework

The Development Framework is comprised of a number of categories that correspond to the Development Framework Plan. Each category embodies different development characteristics and standards, but all categories work in concert to establish a long-term coherent and integrated built form outcome for the Broadway area.

The components of the Development Framework include development standards and guidance for the following areas:

- Mixed Use Corridor
- Mixed Use Shoulder
- Urban Neighbourhood
- Neighbourhood & Neighbourhood Corridor

Heritage Resource & Properties of Interest

Broadway Avenue is not a contiguous heritage environment. However, every effort possible should be made to retain and restore the heritage resources that still exist as they hold great symbolic importance to the entire city and they lend to the novelty and distinction of Broadway Avenue.

Public/Civic Landmarks

The presence of schools and places of worship introduce variety to the streetscape, provide amenity spaces, and lend to the diversity of uses and people in the area. These uses should continue to exist into the future and every effort possible should be made to physically enhance their landmark qualities through restoration, facade improvements and complementary landscaping.

Retail Frontages

A key objective of this Plan is to strengthen and reinforce the retail environment to ensure Broadway's continued success as both a local and regional draw.

Key Corner Sites

Articulating key corners through the massing and architectural design of buildings can enhance the civic quality and image of the area and serves to orient visitors.

Angular Planes Guidelines

In conjunction with height controls are recommendations for massing controls that include the use of angular planes. New developments that are subject to the angular plane guidelines should be massed with good form within these envelopes.

Design Guidelines

The purpose of these Design Guidelines is to provide appropriate design guidance for new development in the Mixed-Use Corridor, Mixed-Use Shoulder and Urban Neighbourhood areas. These guidelines are informed by and reinforce the objectives and standards of the Development and Public Realm Frameworks. Although they seek to provide clarity on intended outcomes of the Plan, they also provide flexibility within certain parameters to encourage distinction, variety and creative architectural responses.

The Design Guidelines address the following building design features:

- Building Expression
- Orientation and Placement
- Street Wall
- Heritage Contexts
- Corner Sites
- Storefronts
- Street Access Units
- Roof Treatment
- Above-Grade Parking
- Material & Architectural Quality
- Sidewalk Cafés
- Building Lighting
- Signage
- Sustainable Design

Implementation (pg. 101)

The Implementation Strategies provide recommendations for putting the Broadway 360° Development Plan into action. Specifically, this section outlines recommended projects, programs, policies or strategies organized according to the three key theme areas that framed the terms of reference for the undertaking of the Broadway 360° Development Plan. As an integrated plan, these themes and their accompanying recommendations overlap in their influence on the long-term objectives for the area.

Land Use

Adopt the Recommended Development Standards

Rather than a complete rezoning, the Nutana Local Area Plan suggests an overlay zone that introduces a tiered approach tied to the scale of development. The Development Framework has been designed to complement a tiered overlay zoning approach and would not preclude harmonization with existing standards or the introduction of additional standards.

Consider an Architectural Control District

With recent changes to the Planning and Development Act, the municipality has been given greater powers to control the design and quality of new developments. As a means for ensuring that new buildings reinforce and enhance the best qualities of the Broadway area, an Architectural Control District can be established to give more weight to adhering to the Design Guidelines.

Atmosphere and Character Retention

Implement Public Realm Improvements

The Development Plan makes a number of recommendations with respect to public realm improvements that serve to enhance the atmosphere and character of the Broadway area. These improvements also serve to reinforce or strengthen desired land use decisions and to direct revitalization where it is needed. Chief among them are extending the Broadway Avenue streetscape vocabulary into all mixed-use areas, the Five Corners Plaza, the Mews, and improvements to the Oskayak School frontages.

Prepare Neighbourhood Infill and Architectural Guidelines

Design guidelines should be prepared that are specifically tailored to Nutana's residential neighbourhoods. They could provide guidance to landowners and developers on appropriate infill, additions and renovations, while assisting City staff in reviewing and assessing applications.

Coordinate Approaches to Addressing Incivilities

There are a number of potential strategies that should be considered for preventative measures and effective enforcement:

- Coordinated enforcement – the Police and Fire Departments and the Saskatchewan Liquor and Gaming Association (SLGA) should patrol together and co-ordinate efforts so when infractions are issued all departments should issue them at once to revoke a license.
- Issue fines not charges – pressing charges requires police officers to return to the station, removing them from the very context that needs their constant presence and surveillance.
- Conditions for licensing – clear conditions should be established for issuing licenses.

Undertake an Awareness Campaign on the Community Benefits of Festivals

There are broad social, economic and profile benefits to successful festivals and events in communities. The benefits to the community should be promoted through an awareness campaign that serves to provide meaningful information but also to generate interest and involvement from the broader community in their execution.

Transportation and Parking

Consider Traffic-Calming Measures to Improve Pedestrian Safety

- The timing for pedestrian crossing at green lights on east-west streets should be increased. Currently they do not provide enough time for pedestrians to comfortably cross within the timeframe given. Increasing the timing will not only make it safer for

pedestrians, but it will also convey the message that pedestrians are important in this area.

- Existing signaled intersections should be fitted with a pedestrian countdown signal to enable walkers to better negotiate their timing for crossing the street.
- The City of Saskatoon should explore the installation of signalized crosswalks and/or bump-outs on streets near Broadway area schools to help improve safety for students and residents alike, provide additional mid-block connections, and help calm traffic speeds.
- Stop signs accompanied by clearly marked crosswalks should be introduced at key, if not all, four-way intersections within the residential areas. This will also help to slow down traffic and discourage motorists from using these streets as through-routes.
- Although the feature pavement treatments and the introduction of other elements in the Mews will serve to slow traffic speeds, other measures such as speed bumps should be considered in other rear lanes to discourage through-traffic and speeds.

Explore Potential Routes for On-Street Bicycle Lanes

A comprehensive study should be undertaken to explore and identify potential on-street bicycle lanes on appropriate streets in the Broadway area, including the potential conversion of the Victoria Avenue Bridge into a pedestrian/cycling only route. Although, the Broadway Avenue Bridge may also be considered as a crossing point for cyclists, a dedicated lane within the commercial area should only be considered if the travel lanes are narrowed and not at the expense of on-street parking or sidewalks. As with all modes of movement, Broadway Avenue should be considered as a destination for cyclists with adequate parking provided.

Better Utilize the Parking Supply

- Force (long term) on-street parkers to leave the area so that the supply is used by patrons of businesses within the area.
- Identify and implement a shared parking strategy for all available off-street parking lots, including schools, such that the peak demands of each are accommodated in different hours of the day. Shared parking is already implemented at the Victoria School and could be extended to include Oskayak School, and businesses on both sides of Broadway Avenue.
- Improve signage and awareness programs to direct visitors to available parking areas, including available areas to the rear.
- Clearly demarcate potential parking stalls to ensure efficient use of available spaces on street – a solution will need to consider all seasons.
- Improve enforcement of parking violations.
- Encourage residents to park in their respective driveways via rear lane and the City should ensure adequate snow removal to do so.
- Implement a parking permit policy program for the residential area, especially where there is a proliferation of rental units. Renters should not compete for the same space that is serving the retail / commercial operations. Their parking demand needs should be solved through their participation in a permit program—even if that permit program has to identify remote parking areas to serve their needs.
- In the medium to long-term as development and re-development occurs, a supply of public parking can be incorporated within one or more parking structures built in association with developments.
- Encourage greater usage of public transit and cycling to and from the area.
- Continue to monitor the parking demand and supply to ensure that it is addressed when it truly becomes an adverse issue for the area.

contents

Acknowledgements

Executive Summary

	1.0	Introduction	1
	1.1	Context	2
	1.2	Purpose	4
	1.3	Report Organization	6
	2.0	The Guiding Vision & Character Areas	7
	2.1	Introduction	8
	2.2	The Five Pillars of the Vision	10
	2.3	The Ten 'Big Ideas'	16
	2.4	Character Areas	19
	3.0	Public Realm Framework	27
	3.1	Introduction	29
	3.2	Streetscapes	30
	3.2.1	Mixed-Use Streetscapes	31
	3.2.2	Residential Streetscapes	31
	3.2.3	Mews	32
	3.2.4	Lanes	32
	3.2.5	Crosswalks	33
	3.2.6	Bicycle Routes	33
	3.3	Open Spaces & Connections	34
	3.3.1	Parks & Green Open Spaces	35
	3.3.2	Plazas	36
	3.3.3	Community Garden & Mid Block Connections	37
	3.3.4	Creating Safe Public Spaces	37
	3.4	Civic Elements	38
	3.4.1	Gateways	39
	3.4.2	Art Sites	40

contents



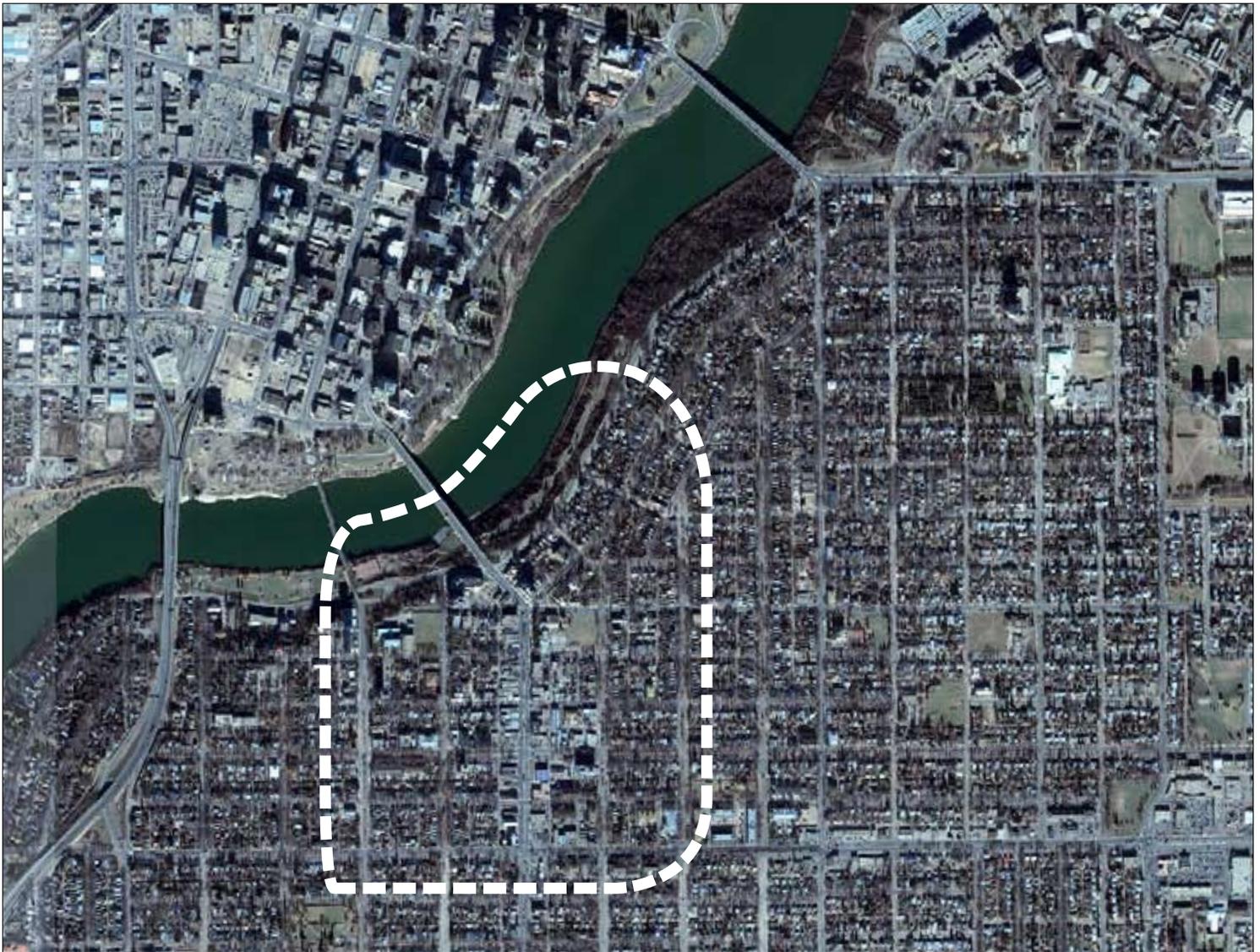
4.0	Development Framework & Design Guidelines	41
4.1	Introduction	42
4.2	Mixed-Use Corridor	46
4.3	Mixed-Use Shoulder	56
4.4	Urban Neighbourhood	64
4.5	Neighbourhood/ Neighbourhood Corridor	70
4.6	Heritage Resources	72
4.7	Public/Civic Landmark	75
4.8	Retail Frontages	76
4.9	Key Corner Sites	79
4.10	Angular Planes Guidelines	80
4.11	Design Guidelines	84
	4.11.1 Building Expression	85
	4.11.2 Orientation & Placement	86
	4.11.3 Street Wall	87
	4.11.4 Heritage Contexts	88
	4.11.5 Corner Sites	90
	4.11.6 Storefronts	91
	4.11.7 Street-Access Units	92
	4.11.8 Roof Treatment	93
	4.11.9 Above-Grade Parking	94
	4.11.10 Material & Architectural Quality	95
	4.11.11 Sidewalk Cafés	96
	4.11.12 Building Lighting	97
	4.11.13 Signage	98
	4.11.14 Sustainable Design	99



5.0	Implementation Strategies	101
5.1	Introduction	102
5.2	Land Use	103
5.3	Atmosphere & Character Retention	105
5.4	Transportation & Parking	107

1.0

introduction



1.1 context

The Broadway 360° Development Plan study area (Broadway area) is centred on Broadway Avenue between the Broadway Avenue Bridge and South Saskatchewan River to the north, and 8th Street East to the south. The study area extends west to Victoria Avenue and east to Lansdowne Avenue. The study area is approximately 55.7 hectares.

The Broadway area comprises the 'heart', 'town centre' and 'main street' of the Nutana community. Established as a Temperance Colony in the late 19th century, Nutana is one of Saskatoon's most desirable neighbourhoods, due in large part to its historic 'small town' charm, the success of the Broadway Avenue commercial area, and the proximity to the Downtown and University of Saskatchewan.



introduction

Purpose

Neighbourhoods are constantly changing and evolving, and Broadway Avenue and the Nutana Neighbourhood is no exception. At the heart of Saskatoon's original neighbourhood, the Broadway area has been the social and commercial core of Nutana for over 125 years. In more recent years, the area's central location, its unique history and character, and the distinct mix of restaurants and stores has attracted new residents and visitors to the neighbourhood, contributing to the revitalization that has made Broadway Avenue a region-wide destination.

Broadway 360° is a comprehensive development plan for the Broadway Area that will help shape future public and private sector decisions and investments, including guidance on the uses and form of development that is appropriate for this area. In essence, the Broadway 360° Development Plan takes over where the Nutana Local Area Plan left off by providing a necessary layer of detail and guidance for the Broadway commercial district and its relationship to the adjacent residential neighbourhoods.

In the spring of 2008, The Planning Partnership was retained by the Broadway Business Improvement District (BBID) and Nutana Community Association (NCA) to undertake Broadway 360°. This planning initiative intended to address a series of long-standing challenges that affect the visitors, residents, tenants

and property owners on Broadway Avenue and in the adjacent residential areas. Although these issues were identified in the Nutana Local Area Plan, they remained unaddressed. It is through this process that the BBID and NCA, with support from the City of Saskatoon, sought to address these neighbourhood-planning issues in a collaborative and proactive manner.

The key intended outcomes expressed by the Steering Committee at the outset of the Broadway 360° process was for a Development Plan that included recommendations on the following:

- A clear and distinct definition of the community's 'ideal Broadway'
- Balanced and appropriate land uses on Broadway Avenue and the surrounding area
- Enhancing and/or reinforcing the atmosphere and character of the Broadway area
- Traffic flow management techniques and ways to develop parking capacity to accommodate growth

In providing recommendations on these broad matters, it is anticipated that many of the outstanding aspects of the Nutana Local Area Plan have been directly or indirectly addressed in this Development Plan.



360° BROADWAY

CREATING OUR NEIGHBOURHOOD PLAN

STEP 2 what do you think about our buildings

STEP 3 what do you think about our streets + open spaces?

STEP 4 what is your vision? for an ideal Broadway

1.2 study process

The Nutana Neighbourhood and Broadway Avenue has a long history of development with numerous citizen and city-led initiatives, each building on the last with the objective of capturing the character that makes this place unique. The Broadway 360° Development builds on those previous studies and was undertaken through a collaborative process over the course of a 10-month, 3-phased process. The process was supported by a Steering Committee comprised of a broad representation of the community, including members of the BBID, NCA, Broadway Avenue landowners and business owners, residential property owners, City staff and the local Councillor.



introduction

Phase One - Background

Phase One consisted primarily of information gathering and review. This phase provided the consultant team with an opportunity to review the relevant plans and studies that were previously undertaken and to begin working with the Steering Committee on preparing for the public consultation events.

Two high profile festivals were held in the Broadway area during phase one - the Fringe Festival and the Broadway Street Fair. To capitalize on the festivals, an exhibit was set up featuring interactive display boards which asked participants pointed questions about Broadway Avenue and the surrounding neighbourhoods. The highly interactive exhibits were facilitated by members of the Steering Committee and were very well attended by the public.

Phase One concluded with the Broadway Avenue Summit - a two-day event that included meetings with the Steering Committee, site visits and analysis, and extensive stakeholder consultation focus group sessions. The stakeholders were asked to identify the assets, issues & challenges and their vision for the Broadway Area. The Broadway Avenue Summit concluded with a public open house at the Oskayak School where the general public were invited to hear the outcomes from the existing conditions analysis and stakeholder focus group sessions, and to participate in the interactive display if they had not been able to during the two festivals. The outcomes of the stakeholder consultations informed the existing conditions analysis and the recommendations contained in this plan.



Participants during the Broadway Street Fair

Phase Two - Vision

The second phase of the study was centred around the Visioning Forum - a multi-day design workshop, where the consultant team set up at the Grace Westminster Church for three days. The Visioning Forum kicked off with a public open house held at Oskayak School. The public open house provided an opportunity to present the initial observations by the consultant team at the outset and generate interest in the study.

The following day, residents and business owners in the area participated in a walking tour of the Broadway 360° study area. The walking tour allowed the consultants to hear about the storied past of the area and some of the issues and challenges, while the residents and business owners had an opportunity to see their community from another perspective.

Over the course of the three days, design options were explored, strategies were discussed, informal public drop-ins were held and plans were revised. The Forum culminated in a closing public open house where the outcomes of the three days were presented back to the community. The participants provided feedback that helped to direct the consultant team in refining and further clarifying the emerging planning directions.

Phase Three - The Plan

The third and final phase dealt primarily with drafting and finalizing the Broadway 360° Development Plan. During this phase, the development options and strategies were refined and a draft Plan prepared for review by the Steering Committee.



(Clockwise from Top Left)
Broadway Forum Walking Tour; Informal drop-in session; Broadway Forum Closing Presentation and Open House

1.3 report organization

The Broadway 360° Development Plan is organized into the following four sections:

The Guiding Vision & Character Areas

This section of the Broadway 360° Development Plan reiterates the vision and objectives for Nutana and the Broadway Avenue area as described in the Local Area Plan. The defining of the 'Ideal' Broadway is then provided in the form of 5 Pillars of the Vision and the 10 'Big Ideas' recommended in the Plan. This section concludes with a description and objectives for the Character Areas that comprise the Broadway Area.

Public Realm Framework

The Public Realm Framework provides guidance for elements in the Broadway Area that are in public ownership - including streets and open spaces. The Public Realm Framework includes guidance for elements such as street character, open spaces, and recommendations for ensuring safely designed spaces.

Development Framework & Design Guidelines

The Development Framework and Guidelines provides direction for the appropriate use, scale and forms of development that is consistent with the Vision, and that will reinforce the objectives of the Development Plan with respect to the Character Areas and improvements to the public realm. It concludes with Design Guidelines that focus on character-defining aspects of new developments such as heritage, storefronts, parking, and material and design quality.

Implementation Strategies

The final section of the Broadway 360° Development Plan provides strategies for implementation. Included are recommendations relating to the outstanding aspects of the Nutana Local Area Plan not fully addressed in previous sections. Accordingly, this section is structured according to three main themes of the project scope: land use, atmosphere and character retention, and parking and traffic.

2.0

the guiding
vision
&
character
areas

2.1 introduction

Broadway 360° Development Plan provides a framework for shaping the future role, function and character of Broadway Avenue and in relation to the adjacent Nutana neighbourhood. The vision, principles and land use objectives defined in the Nutana Local Area Plan have been carried forward and were drawn upon in helping to guide and shape the Broadway 360° Development Plan. They reflect good planning principles that remain relevant and have served as a sound foundation for this study.

They include the following:

Nutana's vision

Nutana's future grows from its storied past and vibrant present: a strong and diverse residential neighbourhood integrated with a strong Broadway core of local businesses and community services. Nutana strives to preserve and strengthen this neighbourhood where residential quality of life is available to young and old, rich and poor, and supports and is supported by a strong town centre. Nutana strives to maintain a community that lives, works and plays in harmony.

The future will see a community that respects and protects its heritage for its direct contribution to our present and future. The community of Nutana will remain a vital, diverse community of residences, businesses, schools, churches, and parks where residents and visitors can walk without fear. Nutana and Broadway will continue to host visitors from across the city and the world in an atmosphere of mutual respect.

vision



Broadway area community convictions

land use goals

A quality residential environment...

- *diversity and balance of housing choices;*
- *single family development;*
- *a diverse population;*
- *lower levels of nuisance and vandalism;*
- *the security to walk in our daily routines;*
- *development that respects and enhances our heritage (cultural, architectural, and natural).*

A strong town centre...

- *a diversity and balance of community services;*
- *viable, vibrant and historic schools;*
- *community meeting places;*
- *businesses that support the everyday needs of its residents and workers;*
- *a safe and unique venue for hosting neighbours and visitors at a reasonable number of events in an atmosphere of mutual respect.*

- *To protect and enhance the mixed use character of the Broadway area;*
- *To protect and enhance housing options in the Broadway Area;*
- *To reduce and control land use conflict between residential and commercial users of the Broadway Area;*
- *To maintain and enhance Broadway as the heart of a historic community;*
- *To protect the human scale of development to promote the street orientation of commercial and residential development;*
- *To foster diversity and the retention of community facilities;*
- *To protect the capability of residential property to deliver basic residential use value;*
- *To manage the festival use of Broadway Area streets to ensure that its street function is not compromised and programmed activities are managed at a level that respects the essential Special Area Commercial and residential character of the area.*



2.2 the five pillars of the vision

A key objective of the Broadway 360° Development Plan is to present a clear and distinct definition of the community's 'Ideal' Broadway Avenue. Informed by the Broadway 360° consultation process and building on the direction in the Nutana Local Area Plan, the following Five Pillars of the Vision and Ten Big Ideas intend to give shape and form to what ought to be the ideal Broadway Avenue to work towards.

vision



1 Towards a Sustainable Nutana & Saskatoon

The Broadway 360° Development Plan is fundamentally about a sustainable way of city-building. Developing sustainably occurs in many ways and should take place at all scales from how a building is assembled to how the city itself is organized and formed. The greatest impact that Saskatoon can make in moving towards a more sustainable future will be in directing compact and mixed-use growth inwards to the Downtown, brownfields, underutilized commercial centres and along main streets. In doing so, farmland and environmental areas are protected, transit is made viable, walking is encouraged, and existing infrastructure and services are more efficiently used.

Nutana is already one of Saskatoon's densest neighbourhoods and would lead most areas with respect to its diversity, mix of uses, and efficient use of infrastructure and amenities. However, the Broadway Avenue corridor remains largely underutilized with a significant number of one-storey buildings and surface parking areas. Greater densities are a planning imperative, given that it is a transit corridor, in close proximity to the Downtown, and afforded with schools, shops and the amenity of the river valley within walking distances.

At the same time Broadway Avenue could benefit greatly from the critical mass of people and activity that more people living and working in the area year-round would bring – a vibrant street life, ensuring the longevity of the area's commercial success, safer streets, and revitalization of areas where it is needed. The Broadway 360° Development Plan sets forth a framework for growth and intensification, one that is balanced, rational and mindful of Nutana's treasured characteristics.



2

Healthy Neighbourhood = Healthy Broadway

Every 'main street' is intrinsically linked to the surrounding neighbourhoods that it serves. A healthy and successful 'main street' is more often than not an undeniable indicator of the health and success of the adjacent neighbourhoods – and vice versa. The reverse is also true, that the destabilization and decline of one would inevitably lead to the undermining of the other. Accordingly, a central focus of the Broadway 360° Development Plan is to ensure a balance between the appropriateness for growth and development along Broadway Avenue and the protection and stability of established adjacent residential neighbourhoods.

Towards this end, the Plan on one hand defines and affirms the character of the neighbourhood areas and encourages that they be maintained. On the other hand, it provides guidance for the scale and form that development ought to take to ensure minimal impact on established low-rise residential areas. Large scaled buildings if inappropriately located and designed can result in significant adverse impacts with respect to shadowing and wind, as well as being visually overwhelming. However, these matters can be greatly mitigated by ensuring that they step down in heights to adjacent neighbourhoods, are massed to give good proportion to streets and are well designed.



vision



3 Leveraging Distinct Character

'Place-making' is an important concept in planning and urban design, which in essence is about ensuring that the development and design of urban areas is rooted in, and draws from, the inherent distinctions of the places in which they are located. Specifically, that the built outcomes are unique, compelling and lend to making the experience of a place one that resonates with visitors and is memorable. People invariably are drawn to distinct places and this is in part a significant reason why Nutana and the Broadway area have become such a desired destination and place to live in Saskatoon. Indeed, across North America, the unique and distinct older neighbourhoods in the centre of cities are experiencing similar revivals as more and more people turn away from the generic, single-use and indistinguishable subdivisions on the periphery.

A key objective of the Broadway 360° Development Plan is character and atmosphere retention. All agree that this place has unique and treasured characteristics that ought to be protected. Moreover, this Plan seeks to also reinforce these features because the success of the neighbourhood and certainly Broadway Avenue, hinges on continuing to leverage these identity-defining assets. Character retention for a commercial area is ultimately about longevity as it can help to ensure that the area continues to attract people and new business.



Through the course of the process, consultation and detailed area analysis, the following ten key elements emerged as fundamental to the distinct character of the Broadway Area that ought to be protected, reinforced and leveraged in the Plan:

1. A great variety and mix of housing lending to a diverse population living in close proximity
2. Diversity and mix of locally owned shops, restaurants and services
3. The charm and distinction of the adjacent historic residential area comprised of a mix of architectural styles
4. Fine grid pattern of street, lanes and blocks that are conducive to walking
5. Magnificent tree lined streetscapes and grand boulevards
6. Intact heritage resources along Broadway Avenue, some dating back to the era of the Temperance Colony
7. Civic landmarks and unique presence of schools on Broadway Avenue itself
8. Human-scaled, street-oriented buildings placed close to the sidewalk
9. Fine-grained scale and rhythm of continuous storefronts that animate the street
10. The proximity to South Saskatchewan River valley and the views of the Downtown beyond
11. The major annual festivals such as the Fringe that draw people from well beyond Nutana's borders

4

Well Mannered & High Quality New Buildings

The form, scale and design of new buildings are important factors in shaping the 'look' and 'feel' of the future Broadway area. The uses, placement, massing, height and quality of buildings on Broadway Avenue need to be considerate of not only how they impact the character of the street, but also other buildings and adjacent neighbourhoods. There is a reciprocal relationship that will need to be in balance, where what the Broadway area offers to the success and appeal of new development, in turn that development should contribute back in reinforcing the best qualities that the area has to offer.

Although taller buildings have greater civic obligations due to their visual prominence and potential impacts, even one poorly designed three-storey building can ruin a street. Buildings last a long time and it is imperative that regardless of scale and location, they be well mannered in their relationship to the public realm as well as to other buildings, and that they be of the highest possible quality. The Broadway 360° Development Plan address the quality of built form, informed by best practices in urban design. Key principles include:

- Buildings should frame streets with good proportion and placed consistently with adjacent buildings
- Active and positive uses should be placed at-grade to animate the street
- Taller buildings should step down to adjacent low-rise areas and heritage resources
- Buildings should be massed to minimize visual and physical impacts
- Appropriate separation distances should be provided between buildings to ensure adequate access to light and privacy
- The design of buildings should express a base, middle and top
- Mechanical areas, loading and parking should be integrated and concealed from view
- Attention should be paid to material and architectural quality, especially at the first three-storeys



vision



5

Pedestrians First

A defining feature of Nutana is its walkability as enabled by the street and block pattern and the proximity to amenities and services within relatively short walking distances. Whether people choose to walk is not the primary concern, rather it is that people have the choice at all. This is an asset that will become ever more valuable as issues of sustainability continue to persist and/or paradigm shifts in the economy or energy take hold.

Broadway Avenue, as with most traditional ‘main streets’ is by design a pedestrian-oriented environment. Indeed, its very appeal, charm and novelty are rooted in the fact that it is an inviting place to walk, stroll, meet and interact. That it is such a vibrant commercial area, despite the lack of any major parking facilities or surface lots, is a testament to people’s willingness to park their cars at some distance and walk to their destination – because the walk is worth the perceived inconvenience. Therefore, making it even more worthwhile is probably a more appropriate and attainable objective, than ensuring an abundance of conveniently located parking lot or uninterrupted traffic movement.

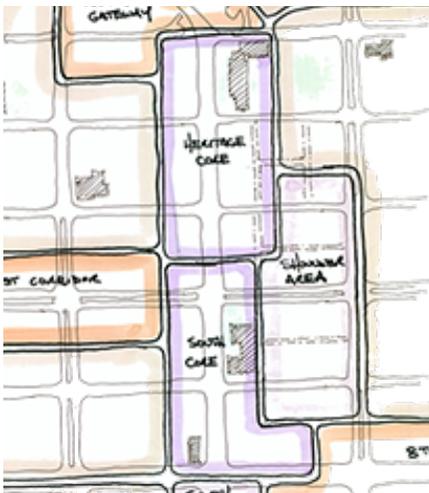
These facts combined with experiences in other places, presents important lessons for moving forward on a Development Plan. Namely, that what is good for pedestrians is good for the neighbourhood and good for business. The continued health and success of Broadway Avenue is as inextricably linked to the quality of the pedestrian environment as it is to ensuring complementary growth and development.

While the area should and must continue to accommodate the car, it shouldn’t at the expense of the quality of the environment as it relates to the convenience, comfort, safety and visual appeal for pedestrians – which extends to transit users and bicyclists as each of these trips also begins and ends with walking. Accordingly, a key focus of the Development Plan in defining the ‘ideal’ Broadway is a care and consideration for pedestrians-first. This translates into great streetscapes, active at-grade uses, human-scale buildings at the street edge, traffic calming, inviting plazas, public art and many other elements that contribute to making it more worthwhile to walk.



2.3 the ten 'big ideas'

The following 10 'Big Ideas' highlight some of the key aspects of the vision and guidance provided in the Development Plan that will shape the future Broadway area. They embody the five pillars of the Vision and point the way towards bringing it these objectives to fruition. Consistent with the Nutana Vision, Community Convictions and land use objectives for the Broadway area, these 'Big Ideas' present the important strategies for further enhancing the area, including elements that serve to address some of the outstanding matters of the Nutana Local Area Plan.



1 5 Character Areas & the Neighbourhood

The Broadway area is actually comprised of five distinct character areas in addition to the surrounding neighbourhoods, which themselves exhibit unique areas of distinction. The Broadway 360° Development Plan seeks to acknowledge and reinforce the best qualities of these distinctions and where necessary provide for appropriate guidance and transitions.

2 An Urban Square with Five Corners

The Broadway 360° Development Plan identifies an opportunity to create an open space focus for Broadway Avenue in the form of a splendid urban plaza at the Five Corners. Framed by existing and future mixed-uses, this proposal consolidates a variety of existing open spaces into a coherently designed plaza that extends feature paving into the roadways to accommodate occasional road closures for major festivals and events.

3 A Relocated Community Gardens

The current and popular community garden on 12th Street is located on a vacant privately owned property that will likely be developed in the near future. An alternate site at the western terminus of 12th Street is identified as a possible location for a new community garden where the design can take the form of a series of terraced gardens embedded into the river valley slope. Proposed to be developed in concert with the new community gardens, are a safe and convenient pedestrian and bicycle mid-block connection and a new public art site at the western terminus of 12th Street.



4 The Mews – Rethinking the Lanes

The Mews correspond to existing commercial lanes that have existing or potential commercial frontages, and which are not shared by residential homes. An opportunity exists to rethink the purpose and design treatment of these lanes to transform them into spaces that in addition to parking and servicing, are also inviting to pedestrians. The Mews can accommodate outdoor patios, enable viable storefronts and galleries, and serve as event spaces for festivals and events.

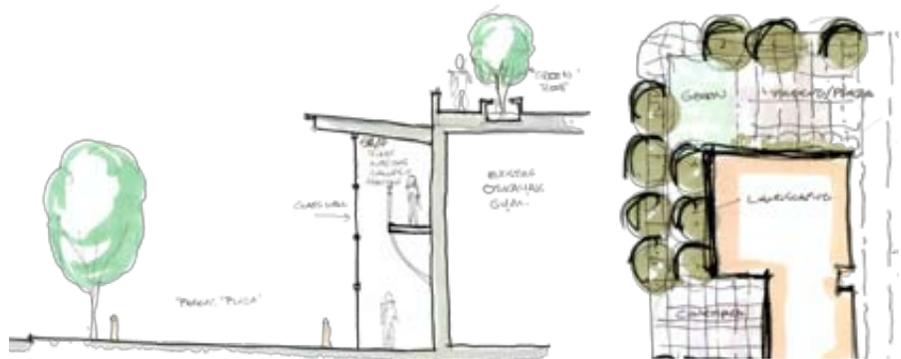


5 Two New Potential Linear Parks

In the future, opportunities could be explored to create linear green park space for passive use and/or playgrounds within street right-of-ways that can accommodate them. For example, Eastlake Avenue between 10th Street and Main Street, between Main Street and 9th Street has no fronting properties and may be transformed into parks to supplement the deficient green spaces in the neighbourhood without adversely impacting traffic movement.

6 A Better Integrated Oskayak School

Oskayak School plays an important function in the community but the building has blank windowless walls facing Broadway Avenue. There exists an opportunity to enhance the quality of open spaces around the building by eliminating the fencing, feature paving the parking area to second as a plaza for events, while seamlessly integrating the green area utilized for pow-wow ceremonies. To enhance the First-Nations presence on Broadway Avenue, the current blank gymnasium wall could even be transformed into a slender and glazed gallery space that can showcase the creative endeavours of the students or broader First-Nations community, while visually contributing to the Broadway Avenue streetscape.



7 The First Three Storeys Matter Most

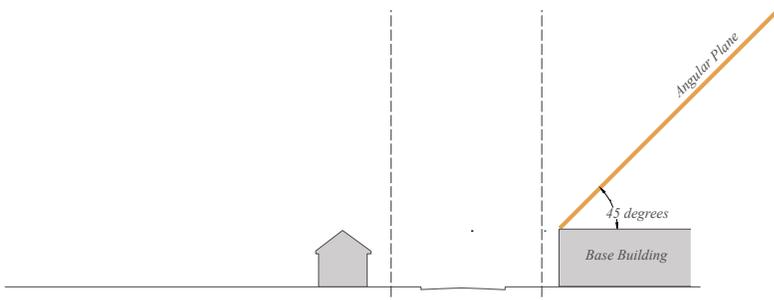
The Development Framework section of the Broadway 360° Development Plan places a great emphasis on the quality and design of the ‘Base Building’ conditions. As the part of the building that frames streets and spaces, and that engages with the sidewalk, the first three-storeys makes the greatest impression on how Broadway Avenue is experienced. Accordingly, specific standards and guidelines are introduced to control the placement, scale, uses and design quality of the part of the building that forms the street wall. Blank walls, non-commercial uses, front-yard parking and gaps in the streetscape should be prohibited along Broadway Avenue.



8

Angular Planes to Ensure Transitions in Height

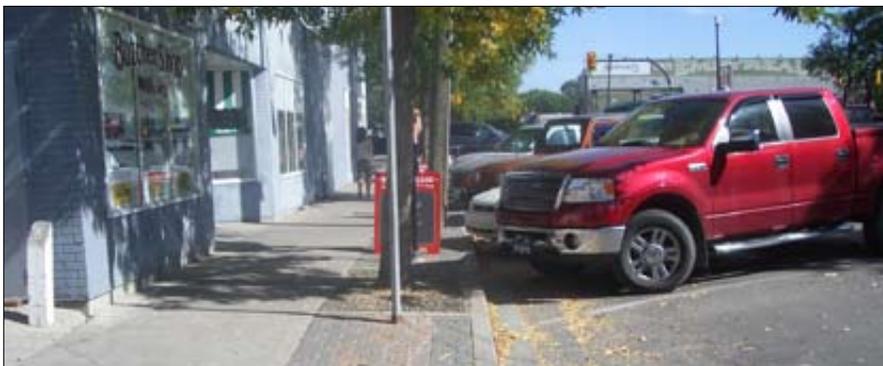
The 45-degree angular plane is commonly used for defining gradual visual transitions, as the vertical rise is equal to the horizontal distance. They are also effective at minimizing overlook and shadow impacts. Generally, the angular plane originates at the nearest residential property line, ensuring that the taller components of the adjacent higher density building are pushed further away. Accordingly, standards are introduced in the Development Plan that utilizes angular planes where developments are adjacent to residential neighbourhood areas.



10

Parking Capacity Can Be Greatly Improved Through Better Management

A key outcome of the Broadway 360° process was the determination that there was more than adequate parking capacity in the Broadway Area. Perceived parking shortages corresponded to a number of issues that included underutilization of the lanes for parking by residents and businesses and the significant numbers of people destined for the Downtown that park in the area. Furthermore, the lack of demarcated stalls resulted in inefficient use of on-street parking areas. In sum, no drastic measures are needed, rather existing parking needs to be better managed and continually monitored. On the other hand, given that provisions for parking is often one of the most costly and constraining aspect for new developments, reduced parking standards can serve as an effective tool and incentive for achieving intended built form and design outcomes. Public parking can also be pursued in association with new major developments in exchange for additional densities where appropriate.



9

Not More than Nine Storeys & With Conditions

For traditional 'main street' contexts, such as the Broadway area, which are surrounded by low-rise neighbourhoods and located outside of the downtown, good urban design principles would suggest that high-rise buildings (as currently permitted) are not appropriate. Maintaining good proportions to the street is a fundamental principle in urban design practice and especially important to shopping and pedestrian areas as it serves to ensure that buildings do not overwhelm the streetscape. Rather, they frame an 'urban room' that provides a comfortable scale of enclosure, while maintaining views of the sky and adequate sunlight. Consistent with the width of Broadway Avenue and existing heights of taller buildings in the area, the Development Plan recommends a maximum height of 30.0 metres, which would enable at most a 9-storey mixed-use building for properties with a minimum frontage of 30.0 metre.




 character
areas

2.4 character areas

Although there is a discernible character and identity for Broadway Avenue and its environs as a whole and that distinguishes it from other places, it is not a homogenous and uniform urban context. Rather, the Broadway area is made up of a number of different parts, each with its own character defining elements. Collectively, these parts all contribute to the identity of Broadway area as a whole. These Character Areas inform a finer level of detail for public realm and built form decisions that build on the unique existing conditions, constraints and opportunities.

Identifying these distinct Character Areas assists in directing certain uses and improvements, as well as guiding appropriate built form responses. The objective is that while each area may maintain its own identity, collectively they complement one another and contribute to the overall vitality and appeal of the Broadway area.

An assessment of the area's history, planning policies, land uses, and built quality reveals that the Broadway 360° area can be defined according to nine distinct Character Areas. Five of these correspond to the Broadway Avenue corridor, while the remaining four correspond primarily to the stable residential areas:

Broadway Corridor Character Areas

- Broadway Heritage Core
- Broadway South Core
- Broadway Village
- Broadway North Gateway
- Broadway South Gateway

Neighbourhood Character Areas

- Main Street Corridor
- Victoria Corridor
- 8th Street Corridor
- Nutana, Buena Vista and Haultain Neighbourhoods

The following section provides built character descriptions and long-term objectives specific to each of the five character areas along the Broadway Corridor and for the combined remaining neighbourhood areas.





Broadway Corridor Character Areas

Broadway Heritage Core



General Character Area Description:

- The Heritage Core Area is centred on Broadway Avenue and generally bounded by 12th Street East to the north and just north of Main Street to the south.
- Defined by the concentration of 1-3 storey buildings, built to the street edge with little to no front or side yard setbacks. The majority of the heritage significant buildings on Broadway Avenue are located in this character area.
- Contains a broad mix of uses with a fine-grained rhythm of narrow, street oriented shops, restaurants and bars, as well as a theatre, offices and Victoria School. This section of Broadway Avenue is where the primary concentration of retail and commercial uses are located.
- Broadway Avenue includes a centre-landscaped boulevard and the streetscaping is well maintained providing an inviting environment for pedestrians.
- The Heritage Core has a clearly defined street edge with no glaring 'gaps' except for an incident of a non-retail use at-grade and parking in front of one property.

Key Character Area Objectives:

- Continue to reinforce and leverage the 'main street' look and feel of the area and the intact heritage resources that lend to Broadway Avenue's distinction and charm.
- Continue to intensify the mix and variety of uses but with sensitivity to the area's heritage character and function and the traditional low-rise street wall.
- Potential infill redevelopment at the southeast corner at 10th Street and retail at-grade introduced where none currently exists.
- Enhance visual and physical connections with Downtown Saskatoon and the South Saskatchewan River Valley.
- Explore opportunities to enhance the existing publicly accessible open spaces associated with the Victoria School frontage.
- Consider the transformation of the service lane on the east side into a pedestrian-friendly Mews that can continue to accommodate parking and servicing, while enabling viable commercial frontages, patios and occasional event spaces for festivals.



Broadway South Core



Key Character Area Objectives:

- Enhance the streetscape by filling in the ‘gaps’ with mixed-uses and at-grade retail to create a vibrant and inviting street life, while ensuring that new development provides appropriate transitions to adjacent neighbourhoods.
- Ensure that new developments contribute to a vibrant street life through the continuation of small-scale at-grade shops and restaurants within a low-rise street wall.
- Enhance the open space and parking areas at the front and north side of Oskayak School to serve as a focus for the area and occasional event space.

General Character Area Description:

- The South Core Area is centred on Broadway Avenue and is generally bound by just north of 8th Street East to the south and just north of Main Street to the north.
- The area contains a broad mix of uses, including some office but mostly shops, restaurants and a grocer. There isn’t a consistent size of shop fronts relative to the Heritage Core.
- Oskayak School is centred on the east side of the street but lacks a positive interface with Broadway Avenue.
- The built form is less consistent than in the Heritage Core. There are sizeable ‘gaps’ along the street edge as a result of auto-oriented retail formats with visible surface parking.
- The quality of the streetscaping is generally consistent with the Heritage Core and is well maintained, but the street is less inviting to the pedestrian as a result of the poor street edge conditions.



Existing Broadway South Core



Potential Broadway South Core

character areas



Broadway North Gateway



General Character Area Description:

- The North Gateway begins at the foot of the Broadway Avenue Bridge, includes the 'Five Corners' at the intersection of Broadway Avenue and 12th Street East, and extends westerly along 12th Street to Nutana Collegiate (**Refer to Section 3.3.2**).
- There is no consistent built form pattern with a mix of older low-rise commercial buildings and more contemporary apartment buildings that range from 3 to 12 –storeys.
- The area is centred on formal and informal gathering spaces comprised of the paved areas formed by the 12th Street closure, as well as the landscaped setback of Victoria School.
- Two surface parking lots terminate 12th Street on the east and west sides of Broadway Avenue.
- There is a vacant site that is presently being used by area residents as a community garden on the north side of 12th Street East and west of Broadway Avenue.

Key Character Area Objectives:

- A transition area from the mixed-uses along Broadway Avenue to a medium and high density residential area, afforded with tremendous views of and access to the river valley and Downtown.
- Strengthen the area's gateway function and enhance pedestrian connections to the Broadway Avenue Bridge, South Saskatchewan River Valley and to a potential new community garden at the westerly terminus of 12th Street East.
- New infill development on vacant and underutilized sites that provide a low-rise street wall with at-grade retail along Broadway Avenue and street-access residential units along 12th Street to be consistent with the existing residential uses on the south side.
- Consolidate the existing open spaces and parking areas at the Five Corners to create a potential plaza and focus for Broadway Avenue with a consistent design vocabulary and feature paving treatments that extend into the roadways.



Broadway South Gateway



General Character Area Description:

- Generally located around the intersection of Broadway Avenue and 8th Street East.
- There is no consistent built form identity, with buildings ranging from automobile service stations at the northeast and southeast corners, 1 and 2-storey commercial and office buildings, and a vacant site at the southwest corner of Broadway Avenue and 8th Street East.
- St. Joseph's Rectory is located at the northwest corner of Broadway Avenue and 8th Street East, which is a prominent landmark as one approaches Broadway Avenue from the south.
- The area serves as a transition in scale and commercial intensity between the north and south sides of 8th Street.
- The quality of the streetscape is similar to the South Core Area but diminishes in quality as one moves south along Broadway Avenue. While the sidewalks are well designed and maintained, the numerous 'gaps' and the lack of street trees contribute to a weak street edge.

Key Character Area Objectives:

- Strengthen the area's gateway function and enhance visual and physical connections along 8th Street East and south on Broadway Avenue by extending the streetscape treatments.
- Enhance visible paved areas associated with the church, which can also function as occasional event spaces.
- Infill underutilized and vacant sites with mixed-uses that provide retail uses at-grade and within a low-rise street wall.
- A potential focus for larger format services and retail that serves the neighbourhood.
- Building heights should step down to the neighbourhoods.



character areas



Broadway Village

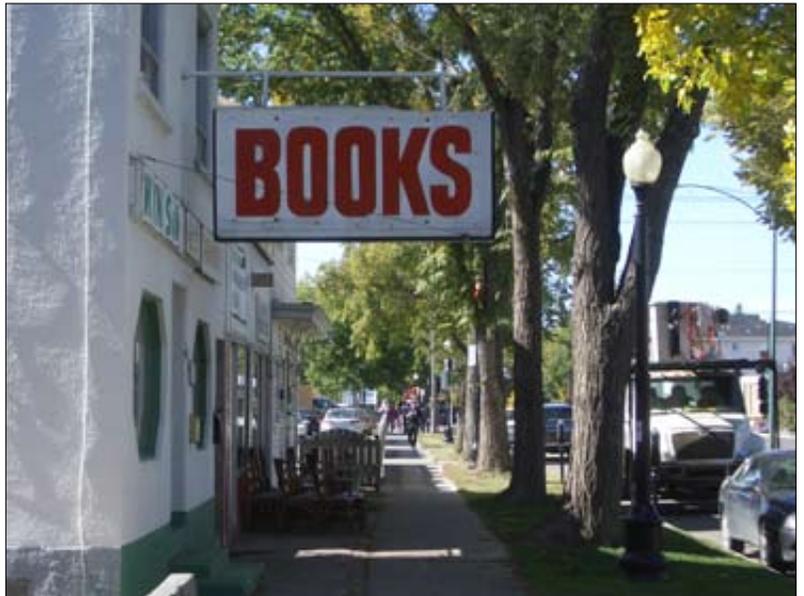


General Character Area Description:

- Defined by a mix of building types, including one to two-storey commercial structures and house forms.
- A mix of land uses and range of residential types, including medium and high-density residential, retail, office and restaurants.
- Setbacks vary, with a large surface parking area consuming much of the grade-level of the Nutana Towers.
- Quality of the streetscape is inconsistent and is especially unappealing where the centre boulevard has been replaced with angled parking on Dufferin Avenue.
- Overall this area lacks a strong and distinct identity and appears to be in a state of transition.

Key Character Area Objectives:

- Potential mixed-use area with 'village' qualities that provides a transition between the intensity of commercial uses on Broadway Avenue and the neighbourhood to the east.
- Can serve as a higher-density residential area that provides larger format shops and services that serve the neighbourhood.
- Infill and redevelopment of underutilized sites such as the car wash and 1-storey commercial buildings to better define and engage with the street.
- Reintegrate Nutana Towers into the streetscape with potential at-grade uses at the street edge and/or with enhanced landscaping and paving treatment.
- Broader landscaped setback and low-rise street walls that include retail or street-access residential uses that lend to a more 'village' character.
- When possible, restore the centre boulevard along Dufferin Avenue to serve as a green buffer and amenity between the mix of uses in the area and the neighbourhood to the east.
- Ensure that new development provides a transition in terms of scale and intensity towards the adjacent residential neighbourhoods.



Neighbourhood Character Areas

The neighbourhood character areas are comprised of predominantly low-density and medium-density residential areas within and around the Broadway area. They include:

- Main Street Corridor
- Victoria Corridor
- 8th Street Corridor
- Nutana, Buena Vista and Haultain Neighbourhoods

General Character Area Description:

- Neighbourhood Corridors are comprised of a wide variety of residential types from house forms, to low-rise apartments, to mid-rise apartments up to 12-storeys.
- The Neighbourhoods are predominantly comprised of house form buildings that are mostly detached, however there are also examples of other low-rise forms such as townhouses.
- Convenience commercial uses also exist at some key intersections such as Victoria Avenue and Main Street, and Temperance Street and Lansdowne Avenue.
- Businesses operating out of homes is also evident, particularly on areas near or adjacent to Broadway Avenue.
- Institutional uses such as churches and schools exist throughout.
- In general, the house forms throughout the neighbourhood areas are relatively older than the higher density forms, many of them of heritage significance.
- Relatively consistent but generous landscaped front yard setbacks are a defining feature, with most homes having porches.
- Many of the streets are characterized by large mature elms that in some cases provide a completely covered green canopy over the streetscape.



Key Character Area Objectives:

- Change in neighbourhood areas should be modest and of a scale, form and type that reinforces the existing built character.
- Protect and enhance the heritage resources that exist, recognizing their cultural value to the history of both Nutana and the City of Saskatoon.
- In Neighbourhood Character Areas, permit infill development that is consistent with existing historic residential building types, similar setbacks, roof lines and material palettes.
- In Neighbourhood Corridor areas permit a variety of higher-density infill developments consistent with existing forms between 3 to 4-storeys.
- Locate garages and access to parking through the rear lanes.
- Explore new opportunities for public spaces to enhance the area's liveability.
- Maintain on-street parking on all streets to calm traffic and provide a buffer to pedestrians.
- Provide stop signs and crosswalks at intersection to enhance pedestrian safety.



3.0

public realm
framework

FIGURE 2: PUBLIC REALM FRAMEWORK PLAN





3.1 introduction

The Public Realm Framework defines and guides the Broadway 360° Development Plan with respect to the existing and potential elements that are primarily in public ownership. This includes public uses, open spaces and streetscapes. This Public Realm Framework is central to the Development Plan as it shapes the most prominent and visible aspects of the urban environment and therefore makes the greatest impact on the image of the Broadway area as a whole.

Consistent with the Vision and Character Areas, the purpose of this Framework is to ensure that the quality and character of the public realm and how it is experienced will enhance the Broadway area's liveability, economic vitality, aesthetic quality and pedestrian environment. The key objective of the Public Realm Framework is to cultivate and nurture a legible, coherent and appealing physical setting by identifying and coordinating aspects of the public realm for improvement and strengthening.

By articulating the objectives for the public realm that are necessary for bringing the long-term vision to fruition, this Framework serves as the primary guidance for informing and making decisions with respect to capital improvements and strategies. The components that comprise the Public Realm Framework are organized according to the following overarching themes:

- Streetscapes
- Open Spaces & Connections
- Civic Elements



Existing high quality streetscaping and street furniture on Broadway

3.2 streetscapes

A streetscape strategy is a central component to any development plan as it can most profoundly define the quality and character of the built environment. Streets comprise the most significant land area in public ownership and are the primary way in which people experience any given place. More than a road, a streetscape defines and considers all of the elements that combine to shape how a street is experienced, including sidewalks, trees, lighting, furnishing, signage, and the character and quality of the buildings that define the street wall.

Given that a culture of walking is so fundamental to achieving a successful commercial area, streetscapes ought to be designed to balance their pedestrian, transit, land use and civic functions, in addition to their vehicular function. Accordingly, a shift in conventional thinking about road design must take place, which considers their character as important as their capacity for traffic.

Certainly Broadway Avenue, but also other streets across Nutana and Saskatoon in general have great 'bones' when it comes to streetscapes. The interconnected grid pattern of streets and blocks, grand boulevards and magnificent elm trees lend to the area's splendid public realm. Furthermore, Broadway Avenue exhibits an exceptionally good quality streetscape environment that is not just a function of the relatively recent improvements (2000), but also an evident attention to detail and material quality. This Plan serves to build on and reinforce an already great infrastructure.

General objectives applicable to all streetscapes include:

- All streets should be lined with trees in increments of no more than 6.0 metres to enhance visual quality and provide shade, but also to serve as effective wind breaks given the prairie climate.
- Wherever possible, sidewalks should 'bump-out' at intersections to define on-street parking areas, lessen pedestrian crossing distances, and to provide ample sidewalk widths at intersections for features such as public art, patios, concessions stands and bus stops.
- Both parallel and angular on-street parking should be distinguished in its paving from the roadway to perceptively extend the pedestrian realm and mitigate the width of the roadway.
- Pedestrian convenience and amenities should be of paramount consideration with respect to ensuring adequate sidewalk widths for the desired volume of use and in the siting, design and quality of furnishings, lighting, bus stops and shelters.
- In commercial areas, patios are highly encouraged to further animate the streets in the warmer months.
- Visual clutter and unnecessary obstructions on sidewalks should be avoided and streetscape improvements should continue to integrate lighting, furnishings, newspaper boxes, trash bins, signage etc. within a unified design vocabulary.
- Existing centre boulevards are treasured aspect of the area's distinction and lend to the 'green' amenity - they should never be dismantled and when and where possible reintroduced.

The following section identifies the key specific design objectives for a variety of streetscape related elements in the Broadway area. The Built Form Framework also reinforces the quality of the streetscapes with respect to guiding how buildings should properly frame and interface with sidewalks and streets.



3.2.1 Mixed-Use Streetscapes

Mixed-Use Streetscapes are streets or segments of streets that generally serve a more intensified mix of uses, including at-grade retail. These characteristics lend to a more 'urban' cross-section as the streetscape should accommodate and encourage high volumes of pedestrian movement and buildings that have a prominent interface and address at the sidewalk. The streetscape of Broadway Avenue is exemplary of this character and similar treatments should be extended to the other identified Mixed-Use Streetscapes to strengthen the cohesiveness of the commercial areas.



Broad distinctively paved sidewalks

Defining characteristics include:

- broad, distinctly paved sidewalks and crosswalks;
- sidewalk bump-outs at intersections;
- unique lighting, banners, signage and furnishing;
- continuous street trees, flower beds other feature landscaping;
- on-street parking;
- mixed-uses with at-grade retail and office or residential uses above grade; and,
- spill-out activity on the sidewalks such as sidewalk patios.



Existing sidewalk bump-out

3.2.2 Residential Streetscapes

Residential Streetscapes are the remaining streets in the Broadway area that primarily serve the residential neighbourhood. These streets serve as important vehicular and pedestrian connections for local residents, and supplement the on-street parking needs of Broadway Avenue. A defining characteristic of these streets is the 'green' amenity they provide through continuous street trees, landscaped side boulevards and/or centre boulevards, and generous landscaped front yards afforded by broad building setbacks. These defining qualities should be protected and enhanced.



Existing mature tree canopy is one of the defining characteristics of residential streetscapes



3.2.3 Mews

The Mews (**Figure 2**) correspond to existing commercial lanes along the Broadway corridor that have existing or potential pedestrian-oriented functions such as commercial frontages, and which are not shared by residential homes. An opportunity exists to rethink the purpose and design treatment of these lanes to serve as 'shared spaces' for pedestrians and vehicles. By formalizing these lanes with design treatments to transform them into Mews, they can provide:

- An inviting environment for pedestrians to enable new potential frontages for retail, galleries and restaurants.
- Alternative and potentially more affordable commercial spaces than on Broadway Avenue.
- More space for spill-out activity such as patios.
- Traffic calming and natural surveillance to enhance safety and security.
- Formalized space to accommodate occasional events and festivals.
- Improved visual appearance and physical linkages for the area.



Examples of Mews with distinctive paving across the entire roadway

The Mews should be designed as 'shared spaces' to continue to permit servicing for the businesses on Broadway Avenue including parking, while being inviting to pedestrians. Shared Spaces are streets/spaces where there are no clear delineations between where cars are permitted to go and where pedestrians are permitted. This can be achieved through an enhancement in the quality of the streetscape that includes:

- distinctive paving across the entire roadway extended from building face to building face;
- enabling and encouraging store fronts;
- enhanced lighting and where possible, street furnishing; and,
- fixtures such as bollards that can close segments of the Mews from vehicular traffic for occasional street festivals, markets and other events.

3.2.4 Lanes

Nutana benefits greatly from the existence of rear lanes (**Figure 2**). Although relatively common in the historic cores of cities in the east, they tend to be more integral to the cities in central and western North America. Although lanes have been diminishing for many years in new suburban developments, they are once again being reintroduced. It is now widely recognized that lanes provide significant benefits that include:

- Accommodating utilities and garbage, directing them away from the streetscape.
- Eliminates curb cuts for driveways on the street, enabling continuous trees and safer pedestrian environments.
- Opens up the front of the homes to better address the street, provide porches and enhances sense of security through natural surveillance.
- By enabling continuous on-street parking in addition to parking in the lanes, far greater parking capacity is possible.
- Provides for safer play areas for children and mid-block connections for pedestrians.
- Enables access for potential accessory units.

This plan encourages the maintenance and better utility of the lanes in the Broadway area. Residents should be encouraged to revive their use as primary places for parking, and the City should provide adequate snow removal to enable this to happen. Every effort should be made to ensure that lanes are adequately maintained and well lit.



Existing laneway on the east side of Broadway Avenue



3.2.5 Crosswalks

The success of a walkable neighbourhood and certainly a shopping street hinges on how well it can accommodate and enhance the pedestrian experience. A sense of comfort and safety will be heightened when the speed of traffic is reduced and direct routes to destinations are provided.

Crosswalks (**Figure 2**) serve two functions: the clear demarcation of a safe route for a pedestrian to cross and as a traffic-calming measure. Frequent stops will ensure slower traffic speeds and cautious driving.



Examples of crosswalks that are clearly identified through distinctive paving treatments

Accordingly, this Plan encourages the following objectives for crosswalks:

- Every key four-way intersection should have crosswalks supported by signaled or signed stops for vehicular traffic.
- To enhance their visibility and quality, crosswalks should be adequately identified and should utilize distinctive feature paving.

3.2.6 Bicycle Routes

Bicycles are a key part of the emerging initiatives for active transportation choices in cities all over the world. Even in winter cities, there has been a significant growth in commuter biking as it is a broadly recognized alternative to the car that is healthier, sustainable, more affordable and often a faster means of getting about where traffic congestion exists.

Where possible, on-street bicycle lanes should be considered for key routes across Nutana and between this area and other neighbourhoods, especially to the Downtown. Bicycle lanes demarcate areas of roadway that are intended specifically for the use of cyclists. They help to increase safety by minimizing conflicts with pedestrians and vehicles through a dedicated right-of-way. In this context it is preferable that these routes be directed to streets that run parallel to major vehicular routes. This is especially important for Broadway Avenue where space is constrained and conflicts with pedestrians is possible.

Furthermore, the introduction of bicycle lanes that links with the existing network of city-wide recreational trails as those along the river valley presents an opportunity to enhance connections with the surrounding community and provides visitors to Broadway Avenue with an alternative to driving. Providing an adequate amount of bicycle parking along Broadway Avenue will be essential to promoting bicycling.



Clearly marked on-street bicycle routes

3.3 openspaces & connections

Open spaces that are successful can have a tremendous impact on the image, appeal and economic development of an area. They are also fundamental to the liveability of a neighbourhood and to attracting continued residential growth.

In 2007, it was estimated that Nutana has a 9.56-hectare deficit in parks and open spaces, excluding the South Saskatchewan River Valley or open spaces associated with schools. However, Broadway 360° recognizes that given the limited opportunities to expand conventional park space, creative opportunities for improving and expanding open spaces should be considered. This begins with understanding that the open space network in traditional urban neighbourhoods can be comprised of not only parks, but also of school fields, courtyards, forecourts, plazas, pathways, and of course streetscapes.

This Plan identifies a variety of existing and potential open spaces that collectively can provide residents and visitors with a variety of choices, appeal to a diversity of interest, and reinforce the vibrancy of the commercial areas. Supported by other plans in the Public Realm Framework, open spaces in the Broadway area can be improved and expanded upon through a series of strategies:

- Improve and better utilize existing open spaces by improving visual and physical linkages.
- Identify new and strategic open space opportunities that can serve as catalysts for revitalization and address areas deficient in open spaces.
- Ensure all open spaces are designed so that they will appeal to the broadest demographic including active and passive, soft and hard, formal and informal spaces.
- Enhance visual and physical connectivity to existing and potential open spaces, including the South Saskatchewan River Valley, through streetscapes, pedestrian connections and orienting features such as public art.
- Ensure that open spaces are designed to be safe and in accordance with CPTED principles.
- Ensure private amenity spaces such as roof gardens and encourage publicly accessible spaces within new significant developments.
- Ensuring that streets continue to provide open space and 'green' amenity through high quality streetscaping.

3.3.1 Parks & Green Open Spaces

Parks and green open spaces are comprised of the formal parkland as well as the 'green' areas generally associated with institutions such as schools. They typically serve the community's passive and active recreational interests and come in a variety of scales and configurations. In the Broadway area, the bulk of these spaces correspond to parkland along the river valley, Nutana Collegiate, Victoria and Oskayak Schools.



In previous studies it has been recognized that the Nutana Community is deficient in park space and there have been a number of solutions presented. Most of the solutions recognize that most of the community is built out and that opportunities for new green parks and open spaces will be difficult to find. Although there may be opportunities to provide smaller green spaces in association with significant new developments, this Plan focuses on encouraging the retention, enhancement, and full utilization of the existing parks and open spaces, including:

- Enhancing the ‘green’ areas in front of Victoria and Oskayak Schools to positively contribute to the Broadway Avenue streetscape through landscaping and seating and other pedestrian amenities.
- Considering the play fields and other ‘green’ areas associated with the schools as shared amenities for the community by providing for passive seating and other amenities, and where possible by finding friendlier alternatives to chain link fences.
- Enhancements can be made to improve the visual and physical linkages with the South Saskatchewan River Valley and the associated trails and parks, including wayfinding signage, improved streetscape links and crosswalks.
- In the future, opportunities should be explored to create linear park space for passive use and/or playgrounds within street right-of-ways that can accommodate them. For example, Eastlake Avenue between 10th Street and Main Street, between Main Street and 9th Street has no fronting properties and may be transformed into park space for the neighbourhood without any adverse impacts to traffic movement.



Examples of the different types of parks and open spaces that exist within the Nutana Community

3.3.2 Plazas

A plaza is generally a paved open space often associated with a civic or commercial function. They vary in shape and size depending on their purpose, but are generally smaller and more intimate in scale than parks. Plazas generally serve high pedestrian traffic areas and/or where major events need to be accommodated that generate large crowds. Year-round plazas function best when they are framed by highly animated uses such as shops, restaurants and cafés.

The Broadway 360° Development Plan identifies a number of opportunities for new and improved plazas to reinforce the prominence of certain civic or commercial uses as well as for accommodating occasional events. They include:

- Lands in and around the Five Corners which consolidates three existing open spaces associated with ‘The Broadway’ tower, Victoria School and the areas in front of the existing Roastery. This area can become an important focus and gateway for Broadway Avenue, animated by intensified commercial and residential uses. By extending the paving treatment over the roadways, what functions as three small plazas, streets and parking areas during normal times can easily be transform into one large plaza for special events and festivals.

- An opportunity exists for improvements to the staging area in front of Victoria School at the northwest corner of 11th Street and Broadway Avenue.
- Other plaza opportunities include existing parking and paved areas associated with Oskayak School and St. Joseph’s Roman Catholic Church, which are currently visible and detract from the quality of the streetscape. With special paving treatments, these areas can continue to serve their vehicular function but in a visually more complementary way and they can also take on the function of plazas on occasion.



Top: Existing conditions at the Five Corners

Above: Potential Plaza that functions as a parking area (left image), but can be used for festivals and events (right image).

Left: Demonstration Concept Plan for a Public Plaza at Five Corners

(1) Multifunctional spaces that can be adapted for pedestrian-only use during festivals and other events. (2) Future buildings would include storefronts, better defining the space. (3) Removable bollards, rather than curbs, separate auto and pedestrian areas. Consistent use of feature paving helps to unify the space. (4) Potential use of rolling curbs to separate other auto and pedestrian areas. (5) Closure of 12th St. W. to through-traffic produces a larger plaza space. (6) Possible site for street vendors. (7) Potential site for public art. (8) A decorative gateway paving treatment.



3.3.3 Community Garden & Mid Block Connection

The current and popular community garden on 12th Street is on vacant privately owned land that will likely be developed in the near future. As an outcome of the Broadway 360° process, an alternate site at the western terminus of 12th Street was identified as a possible location for a new community garden on land in public ownership. The lands are a significant underutilized green area bounded by the Nutana Collegiate to south, Victoria Avenue to the west, and the Victoria Bridge/Saskatchewan Crescent interchange to the north.

Although a steep grade associated with the river valley, the design can take the form of a series of terraced gardens. In addition to providing a new location for the community gardens, this intervention can also leverage multiple community benefits that can include the formalization of an underutilized green open space, the creation of a safe and convenient pedestrian and bicycle mid-block connection between 12th Street and the Victoria Avenue Bridge, and the opportunity for a new public art installation.



Example of a terraced community garden

3.3.4 Creating Safe Public Spaces

How open spaces are designed, programmed and interface with surrounding streets and land uses is crucial to their capacity to attract and delight a broad a spectrum of users and to ensuring that they are places where people feel safe and comfortable. Accordingly, principles that build on **Crime Prevention through Environmental Design (CPTED)** should be applied to all publicly accessible open spaces in the Broadway area.

CPTED is a pro-active crime prevention strategy utilized by planners, architects, police services, security professionals and everyday users of space. CPTED is a strategy used around the world and in essence, operates on common sense. CPTED advocates the proper design and effective use of the built environment to lead to reduction in the incidence and fear of crime and in the overall improvement to quality of life.

Utilizing and building upon the CPTED policies and principles in **Section 3A.1 of the Saskatoon Development Plan**, Broadway 360° adheres to and encourages following key principles for the design of any publicly accessible open space:

- Ensure clear views of surrounding areas and of the open space, including the removal of low lying shrubs and landscaping in the centre boulevards of streets
- Incorporate adequate lighting
- Active building frontages on the open space, including entries and windows, are critical for eyes on the space, which enhances the sense of safety and security and discourages inappropriate behaviour
- Design the space to enable ease of access and egress and avoid the creation of entrapment spots that are not highly visible and well used
- Use legible signs and orienting devices such as landmarks and pathways
- Include a variety of activities and a mix of surrounding uses to facilitate constant public use and/or surveillance.



3.4 **civicelements**

Civic Elements are comprised of Gateways & Public Art sites, which have been identified in the Public Realm Framework plan. The identification of these elements ensures a coordinated approach to the design of buildings, streets and open spaces so as to reinforce visual connections into and throughout the Broadway area. Both public art and gateway sites provide important cues to the pedestrian as they serve to draw and orient movement through an area by enhancing visual connectivity to important sites and destinations.



3.4.1 Gateways

The Broadway 360° Development Plan identifies potential Gateways. Clearly defining Gateway areas helps to enhance orientation, lends to a sense of place and can generate civic pride. Gateway sites help signal key points of entry into the Broadway area. These locations provide key opportunities where the co-ordinating of the design of landscapes, signage, public art and buildings can create a sense of entry and orientation. The expression of a Gateway can take on many forms and will hinge on the individual circumstances of the site.

The Broadway 360° Development Plan encourages the creation of Gateways according to the following design objectives:

- Gateway experiences should reflect the unique culture, history and environment of Nutana and Broadway Avenue.
- Gateways should provide a significant sense of scale and identity that act as both thresholds to, and landmarks for, the Broadway area.
- Gateways should be well integrated while marking a sense of entry for visitors.
- The built form and public realm context of the Gateway should be held to higher design standards.
- Gateways should exhibit co-ordinate site planning, streetscaping, built form and landscaping to create a unified environment. This can be achieved through:
 - celebrating Gateways with public gathering spaces;
 - locating primary building entrances at gateways;
 - providing visually prominent massing, such as distinct corner or roof treatments;
 - special attention to architectural and material quality;
 - locating public art at Gateways;
 - consistency of materials, colours and textures in built form and landscape (i.e.. building façades and paving materials);
 - providing special streetscape elements or furnishing such as signs, arches, columns, or fountains;
 - consideration for visibility at night and winter months through lighting and vertical expressions; and,
 - ensuring that parking, loading, servicing, utilities, mechanical equipment and other unsightly functions are located away from Gateway locations.



Using signage as a gateway



The design of buildings can serve as a gateway



Public Art or Sculpture as a gateway

3.4.2 Art Sites

Public Art already has a significant and prominent presence in the Broadway area. This demonstrates a local appreciation for the benefits that public art can have on community expression, but also on the appeal and image of the area. The Broadway 360° Development Plan builds on the existing public art resources by identifying other visually strategic locations for potential art installations. The strategic locations identified typically correspond to the terminus of view corridors, at gateways, on prominent corners, and/or in parks and open spaces.

Public Art provides an opportunity to celebrate and showcase local arts and culture; establish a unique identity for the Broadway Area; and, can contribute to enhancing the quality of the public realm in ways that conventional streetscape elements cannot. Objectives for public art include:

- Public art may include memorials, sculpture, water features, murals, or individual art installations at visually prominent sites, open spaces, gateways, along sidewalks and in association with public buildings and school sites.
- Public art sites should be highly visible and as such, these sites should include installations that serve as accents to the Broadway area, as orienting devices for moving about, or as focal points in public open spaces.
- The scale of the installation should correspond to the visual prominence of its site.



Existing Public Art at Broadway Avenue and Main Street



Public Art may include water features and elements

4.0

development
framework
&
design
guidelines



4.1 introduction

The Development Framework builds on and reinforces the Vision, Character Areas and the Public Realm Framework to provide guidance for new buildings in the Broadway 360° area. This section addresses the function, shape and character of buildings, as well as their relationship to adjacent properties, streets and open spaces. The purpose is to define a coherent, rational and integrated structure for guiding the future built quality and character of the Broadway area as shaped by the use, massing, scale, typology and design of buildings.



A key objective of the Broadway 360° Development Plan is to build on the Nutana Local Area Plan to provide an integrated set of recommendations for land use and character retention. While the Development Framework provides direction on the appropriate parameters for development to inform the land use and zoning regime, the Design Guidelines focus on the quality and character of new development, which can provide the substantive content for architectural controls in the area.

Although high-level direction is provided for a larger area to ensure a coordinated approach, most detail is focused on the Broadway Avenue corridor where the greatest magnitude of change can be anticipated and where the greatest guidance is needed. This generally corresponds to areas subject to the existing B5 Zoning District.

Development Principles

A central objective of the Development Framework is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm. At the same time, new buildings should reinforce the best qualities that characterize the Broadway area and ensure appropriate relations with heritage resources and the adjacent low-rise residential neighbourhood. Informed by the consultation process and rooted in good planning and urban design practice, the key principles that underpin the Development Framework include:

- Protecting heritage resources and retaining their visual prominence.
- Protecting and strengthening established low-rise residential areas and ensuring compatible infill development and sympathetic developments in adjacent higher density areas.
- Ensuring base building conditions that form an appropriately scaled and designed street wall that reinforces the desired character at the street level.
- Ensuring appropriate building massing and height taking into consideration existing and permitted heights; proportional relationships to streets; and, visual and physical impacts on pedestrians and adjacent areas.
- Ensuring that new developments providing for appropriate transitions between areas of differing intensities and scales.
- Reinforcing important intersections and corners through massing and design.
- Well designed and articulated buildings that positively contribute to the quality and animation of the streetscape and the overall defining character and image of the area.

FIGURE 3: DEVELOPMENT
FRAMEWORK PLAN





Components of the Development Framework

The Development Framework is comprised of a number of categories that correspond to the Development Framework Plan. Each category embodies different development characteristics and standards, but all categories work in concert to establish a long-term coherent and integrated built form outcome for the Broadway area. That there are differences is reflective of the complexity and transitional nature of this urban environment. The Development Framework recognizes this and seeks to clarify and reinforce this distinction but in as seamless a way as possible. These categories are defined and named to provide a rational and logical structure to the recommended development standards for the purposes of this study and are not intended to reference or substitute current City of Saskatoon zoning or land use designations.

The components of the Development Framework include development standards and guidance for:

Land Use & Built Character Areas:

- 4.2 Mixed Use Corridor
- 4.3 Mixed Use Shoulder
- 4.4 Urban Neighbourhood
- 4.5 Neighbourhood & Neighbourhood Corridor

Special Land Use & Built Character Categories

- 4.6 Heritage Resource & Property of Interest
- 4.7 Public/Civic Landmark
- 4.8 Retail Frontages
- 4.9 Key Corner Site

These sections are then followed by:

- 4.10 Angular Planes Guidelines
- 4.11 Design Guidelines



Example of Neighbourhood Character Area

4.2 mixed-use corridor



The Mixed-Use Corridor areas correspond to properties fronting onto Broadway Avenue between 8th Street East to the south and just beyond 12th Street East to the north. These lands encompass the primary functional, symbolic and historic heart of the Broadway area and the greater Nutana community. They include a mix of land uses, with retail most prominently at-grade and office and/or residential uses above. The existing built form character varies greatly from one-storey commercial pads with parking in front; to traditional one, two and three-storey street-oriented buildings; to mixed-use, mid-rise buildings up to 12-storeys.

Although a 12-storey building prominently occupies the north visual terminus of Broadway Avenue, at 12th Street East, and a 7-storey building is currently under construction at the northwest corner at 9th Street East, the predominant existing scale of buildings is in the range of 1 to 2 storeys. However, depending on the width of the property frontage, current zoning (B5 District) would permit as-of-right developments with densities up to 10 times the area of the lot and up to 76.0 metres in height. This can potentially translate into a mixed-use building of approximately 25-storeys or an office building of approximately 19-storeys.





The Mixed-Use Corridor, along with the Mixed-Use Shoulder and Urban Neighbourhood areas identified in the Development Framework Plan hold great potential for infill and intensification. Given that much of the remaining lands in Nutana are predominantly established residential areas, the substantial growth opportunities for this community will likely be limited to the Broadway Avenue area. As broadly recognized in comparable places across North America and by this community in consultation for this Plan, continued growth directed to the Broadway area could also result in significant benefits that among other things include:

- Infilling vacant lands, 'gaps' in the streetscape and redeveloping properties that do not positively contribute to the area.
- Providing greater housing choices to accommodate diverse incomes levels, life styles and age groups within Nutana.
- Ensuring the continued revitalization and improvement of the area through continued change and enhancements.
- Enhancing the vitality of local business, as well as the vibrancy of the street life, which also serves to improve safety.
- Strengthening the mix of uses and providing residential densities in close proximity to the Downtown, which will encourage active transportation choices – such as walking, transit and cycling.
- Reinforcing a more sustainable growth pattern that directs development to where it can be accommodated and supported by existing infrastructure, amenities and services – such as shopping, schools and public transit.

While growth and intensification has its inherent benefits, its success hinges on how it occurs and what form it takes. A central purpose of this Plan is to provide guidance for new development in a manner that is rational, balanced and appropriate for the Broadway area and Nutana community. Accordingly, the following recommended development standards provide a tailored approach and another level of detail currently not provided in the existing policy and zoning framework. These standards build on the ideas developed and presented at the Broadway 360° Forum and are consistent with best practices in the urban design and planning of comparable areas of other North American cities.



Broadway Avenue looking north towards 10th Street East

Recommended Development Standards

The following are the recommended development standards for Mixed-Use Corridor areas. Where necessary, they are accompanied by supporting contextual commentary and rationale in coloured italics. Where an alternative standard is not specified in this or other sections of the Plan, existing developments standards can be assumed.

Uses

- A broad mix of uses should continue to be permitted, except for automotive related uses such as services stations, drive-throughs and other uses that detract from the quality of the streetscape and pedestrian-oriented environment.
- Retail uses such as shops and restaurants with active frontages (entries and windows) onto Broadway Avenue should be required.

A continuity of animated at-grade uses is essential for the success of a pedestrian-oriented commercial area and blank walls, gaps or non-retail uses at the sidewalk should be prohibited.

- Within the Heritage Core character area in particular, retail uses at-grade should be limited in scale to be consistent with the fine-grained character of existing shops, generally having a frontage in the range of 7.5 metres but not greater than 15 metres.

A key defining character of Broadway Avenue is the fine-grained rhythm of shops that lend to the vibrancy and visual interest of the street. The smaller shops are also more suited to the diverse nature of retailers that includes many local owners over national brands or franchises. Retaining this concentration of store types can be encouraged by limiting their scale and by directing large formats to second levels or to the Mixed-Use Shoulder areas where they can be accommodated.

Base Building refers to a building or the component of a building that occupies the first three storeys and that frames the street. It is what forms the 'street wall' and corresponds to what is defined as the 'Historic Tier' in the Nutana Local Area Plan.

Base Building

- To be consistent with the character of existing mixed-use heritage buildings, grade level heights should be no less than 4.5 metres to enhance the prominence of storefronts.
- To reinforce a consistent and well-defined street edge of a pedestrian scale, the Base Building should be a minimum of 7.5 metres (2-storeys) and maximum of 12.5 metres (3-storeys) in height.
- To ensure a consistent street wall with buildings placed close to the sidewalk, a 'build-within zone' should be established of 0.0 to 0.5 metre for interior lots and 0.0 to 1.0 metres (2.0 metres on the side street) for corner lots. Where an entire block is redeveloped, a setback of up to 3.0 metres should be permitted.

Modest setbacks can enable the widening of sidewalks for pedestrian amenity and spill-out activities such as sidewalk cafes.



- To ensure a continuous building edge, interior side yard setbacks should be prohibited for the first 2-storeys.
- Rear yard setbacks should not be required, however where a Base Building is abutting a property within a Neighbourhood area, it may be subject to above-grade stepbacks in accordance with the Angular Plane Guidelines.
- Where at-grade commercial uses fronting the rear lane are proposed and permitted, a minimum 3.0 metre rear yard setback should be provided.
- At-grade parking should be prohibited from any street frontages and directed to the rear of the Base Building, accessed through the rear lanes.
- While below-grade parking should be encouraged, above-grade parking should be permitted within the Base Building under the following conditions:
 - Above-grade parking areas are included in the density calculation.
 - At-grade uses are provided on all street frontages with no less than 15.0 metres depths from principle streets and no less than 7.5 metres depths from side street
 - The façade treatment of the above-grade parking is subject to the Design Guidelines.
 - Access to parking is provided from the rear lane

Height & Massing

- To ensure a proportional relationship to the street, adequate transitions to adjacent low-rise areas and good urban design at an appropriate scale for the Nutana context, buildings should not exceed 30.0 metres (7 to 9-storeys depending on the uses) in height.

Appropriate building heights could be determined by a number of considerations including existing heights, proximity to low-rise residential areas, scale and configuration of blocks and properties, public realm objectives and the broader urban structure for the city. Conventional wisdom dictates that the tallest buildings should be directed to the city core and major centres, stepping down to minor centres and arterial corridors, with the lowest buildings in traditional residential neighbourhoods.

For traditional main street contexts surrounded by low-rise neighbourhoods and outside of the downtown such as the Broadway area, good urban design principles would suggest that high-rise buildings as currently permitted are not appropriate. Rather, building heights ought to be dictated by two key objectives: providing for appropriate relationship to adjacent low-rise residential properties, and maintaining a good proportional relationship in height to the scale of Broadway Avenue itself - generally a 1:1 ratio of building height to street width.

Maintaining good proportions to the street is a fundamental principle in urban design practice that one can experience in many well-visited cities including Paris or London. This proportional relationship is important to shopping street and pedestrian areas as it serves to ensure that buildings to

These standards address the components above the Base Building where developments taller than 3-storeys are proposed. These standards address what is defined as the High Density Tier in the Nutana Local Area Plan.

not overwhelm the street. Rather, they frame an 'urban room' that provides a comfortable scale of enclosure, while maintaining views of the sky and adequate sunlight. As Broadway Avenue is a 30.5 metre right-of-way, a 30.0 metre height maximum that is comparable to a 9-storey mixed-use building would be appropriate.

- To ensure a street wall height that is consistent with the heritage character of the Broadway area, building components above the Base Building should provide a minimum 3.0 metres setback from the face of the building that is the street wall. Modest exceptions may be permitted on Key Corner Sites.

In addition to reinforcing the historic street wall heights, this significant setback serves to mitigate wind impacts at the sidewalk that may be caused by the sheer walls of taller buildings.

- To minimize visible above-grade blank wall conditions and to provide for adequate separation distances between taller buildings, a minimum 6.0 metre rear and interior yard setback should be required for all buildings taller than 5-storeys.

This setback can ensure that windows can be provided at the sides of buildings while providing for adequate light and privacy to facing units or offices. Furthermore, this separation distance ensures a rhythm of breaks between buildings to enhance sky views and enable greater sun penetration.

- A minimum 18.0 metre separation distance between the principle faces of residential buildings taller than 5-storeys should be provided to ensure adequate light and privacy.
- To ensure appropriate transitions to, and interfacing with, adjacent properties within Neighbourhood areas, the massing of buildings should be subject to the Angular Planes Guidelines.

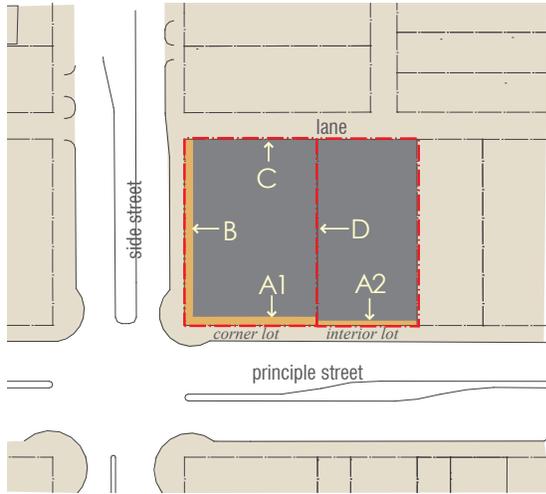
The 45-degree angular plane is commonly used for defining gradual visual transitions, as the vertical rise is equal to the horizontal distance. They are also effective at minimizing overlook and shadow impacts. Generally, the angular plane originates at the nearest residential property line, ensuring that the taller components of the adjacent higher density building are pushed further away. When applied to the depth of a typical property along Broadway Avenue (approximately 42.5 metres or 140 feet), a height of 9-storeys is achievable in a standard residential floor plate configuration.

- To minimize excessively massed buildings, floor plate dimensions for levels above the 5th storey should not exceed 35.0 metres.
- To ensure buildings that are consistent with these standards and of good and viable form, the following minimum frontage standards should be applied:
 - Frontage of less than 15.0 metres = 3-storey maximum height
 - Frontage between 15.0 metres and 30.0 metres = 5-storey maximum height
 - Frontages of 30.0 metres or greater = 9-storey maximum



Summary of Key Recommended Development Standards

base building placement



Buildings should be placed within the shaded area as shown.

- A1 Front Setback for corner lots: 0m-1.0m
- A2 Front Setback for interior lots: 0m-0.5m
- B Side Street Setback: 0m-2.0m
- C Rear Setback: Not required
- D Side Yard Setback: Prohibited

Build within zone: Allows for widened sidewalks for pedestrian amenity and spill-out activities such as sidewalk cafes.

parking



Vehicular access permitted only from rear lane. At-grade parking (enclosed and unenclosed) should be placed in the shaded area shown.

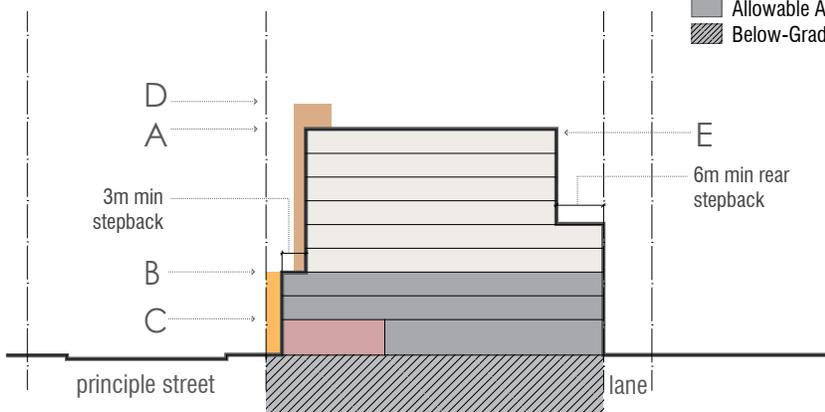
- A Front Setback: 15.0m minimum
- B Side Street Setback: 7.5m minimum
- C Rear Setback: Not required
- D Side Yard Setback: Not required

Required at-grade retail

building profile

- A Maximum height: 30.0m
- B Streetwall: Minimum 7.5m to Maximum 12.5m
- C Minimum grade-level height: 4.5m
- D Key Corner Sites: Up to 3.0m allowed above maximum height, maximum 5.0m frontage on either street face
- E Massing may be subject to angular plane

- Required At-grade Retail
- Allowable Building Encroachment for Key Corner Sites
- Build within zone
- Allowable At-grade and Above-grade parking
- Below-Grade Parking Zone



Not all properties can accommodate taller buildings in a viable and good form while meeting these development standards as well as provisions for onsite parking. Accordingly, these frontage requirements are consistent with the scale of property necessary to achieve the corresponding scale of development. Given that very few properties currently exist that can accommodate the maximum 9-storeys, most development will likely remain below 5-storeys unless significant consolidation occurs.

Gross Floor Space Ratio (FSR) is a measure of the total amount of building floor area relative to the area of the property. Although density is not an adequate mechanism for defining the shape and form of development, it is an appropriate means for controlling the overall scale of a building relative to its site.

Density standards should be tied to contexts rather than property frontage. Being a ratio that directly corresponds to the scale of the property, it is unnecessary to differ the ratio from property to property within a context where more uniformity in building forms is desired.

Development Density

- To ensure appropriately scaled developments for the Broadway area context, the gross floor space ratio should not exceed 5:1.

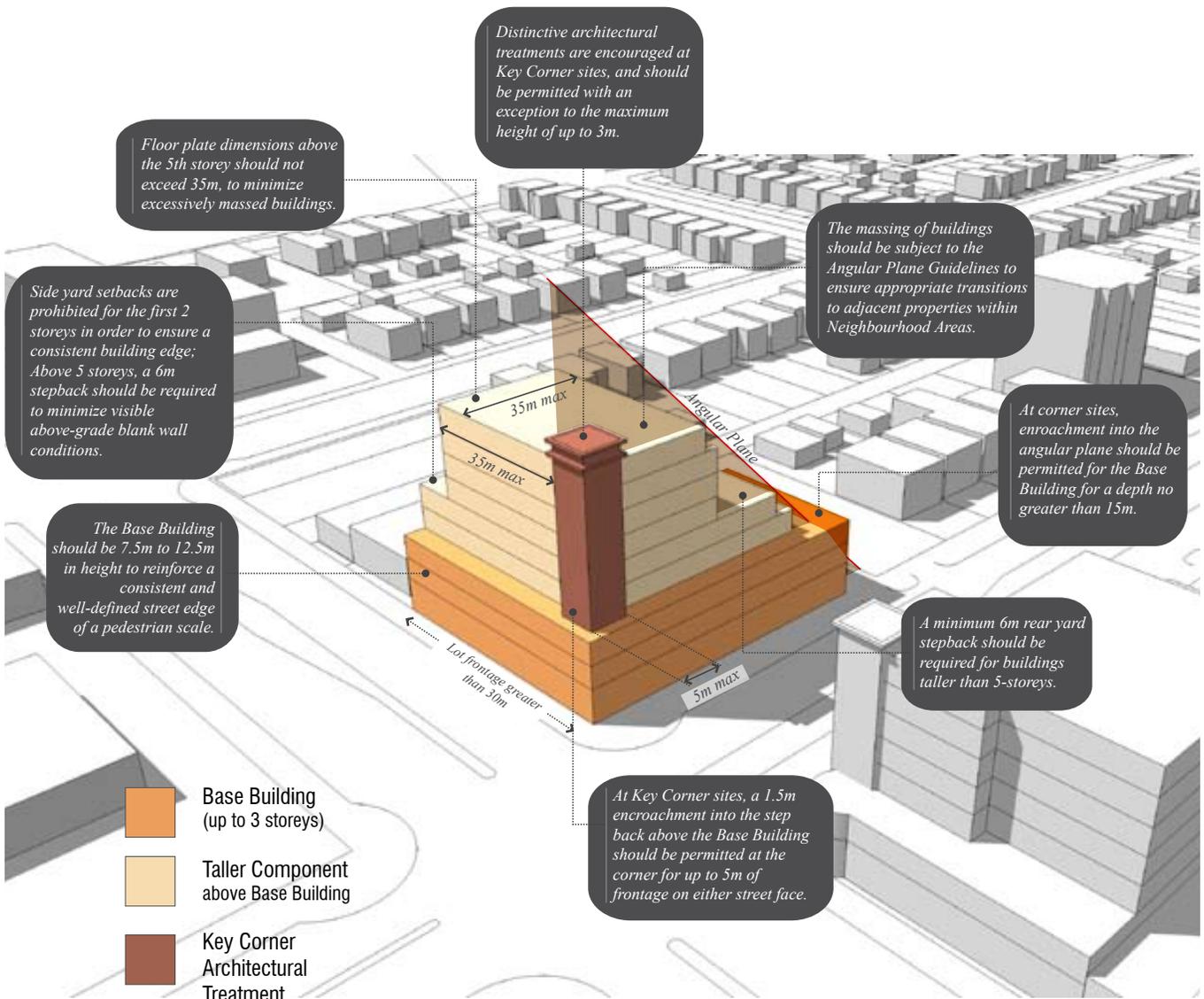
FSR should work in conjunction with other development standards to encourage desirable built form outcomes. Accordingly, the 5:1 FSR was determined to be an appropriate standard as an outcome of the testing of the recommended standards with respect to heights, setbacks, stepbacks and massing within the angular planes. Furthermore, this is a density standard that is generally consistent with best practices in other comparable contexts that include mixed-use corridors adjacent to established residential neighbourhoods.

- To provide an incentive for certain public benefits such as heritage retention, affordable housing, public parking, contributions to public realm improvements or other priorities determined by the community, a density bonus should be considered for sites where it can be accommodated and where all other development standards are complied with, including heights and angular planes.

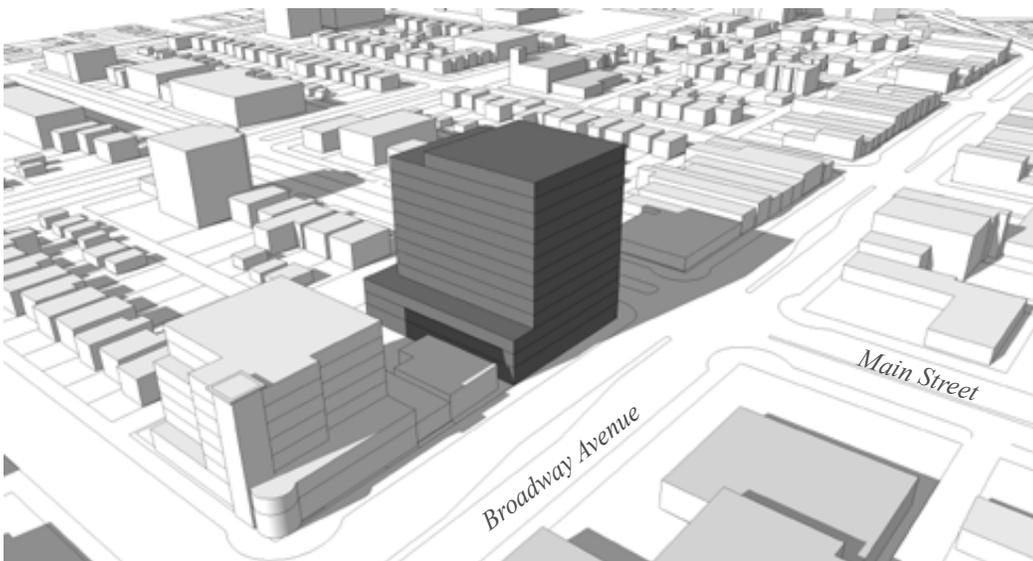
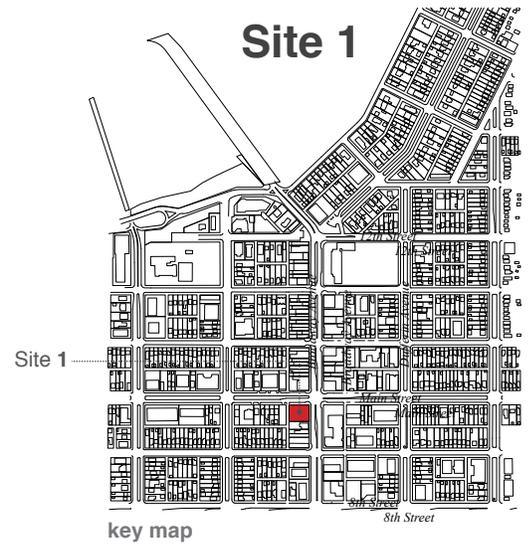
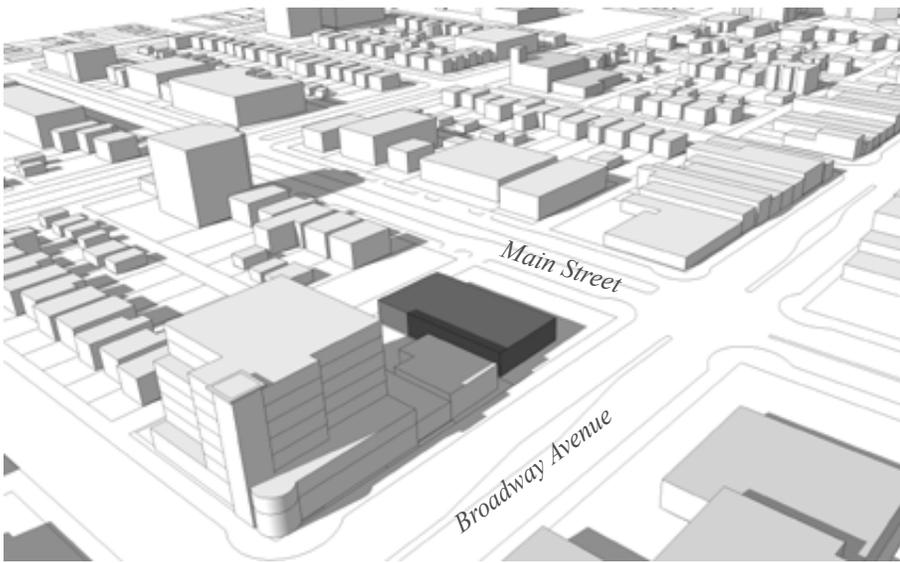
Not all developments will be able to achieve a 5:1 FSR depending on the scale of the property and the angular planes it may be subject to. However, where developments are proposed on larger scaled properties that are less constrained (such as by angular planes), there may be opportunities for greater density that can be deployed in a manner that remains consistent with all other standards. However, this additional density should only be considered as bonus in exchange for community benefits. To ensure consistency and predictability, the amount of density should directly correspond to a formula such as a square metre of bonus density for every square metre of heritage building retained or affordable housing provided.



Illustration of the application of the recommended development standards for Mixed-Use Corridor Areas



Demonstration of Recommended Development Standards



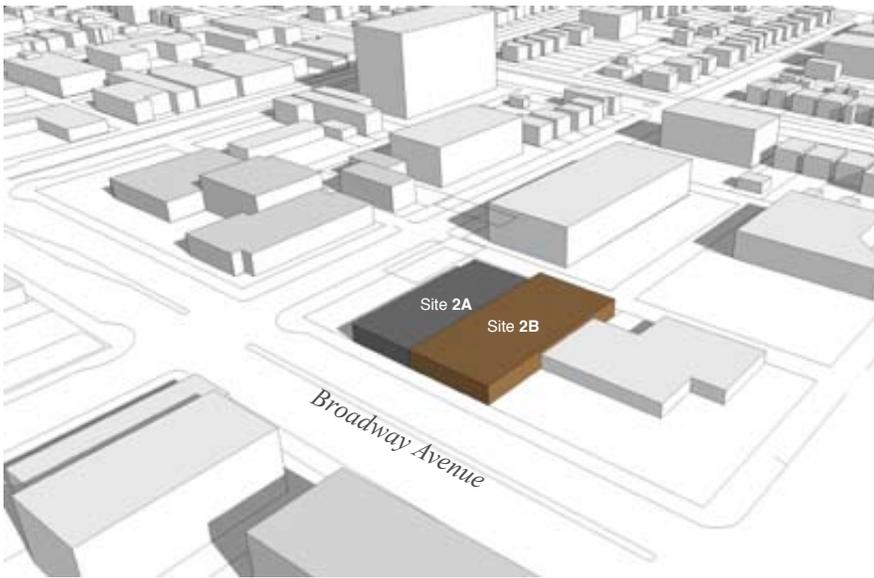
Current Zoning Standards

Site Area	1,515 sq.m
Frontage	35.5m
GFA	15,150 sq.m
FSR	10.0
Height	13 storeys
	(40.0m)

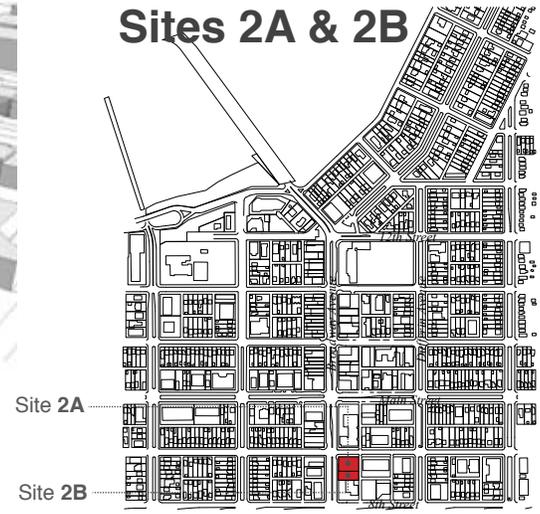


Proposed Standards

Site Area	1,515 sq.m
Frontage	35.5m
GFA	7335 sq.m
FSR	4.8
Height	9 storeys (28.5m)



Sites 2A & 2B



key map



Current Zoning Standards

Site 2A (Corner Site)

Site Area	1,300 sq.m
Frontage	30.5m
GFA	13,000 sq.m
FSR	10
Height	13 storeys
(40.0m)	

Site 2B (Interior Lot)

Site Area	810 sq.m
Frontage	19.0m
GFA	5,670 sq.m
FSR	7.0
Height	8 storeys (25.0m)



Proposed Standards

Site 2A (Corner Site)

Site Area	1,300 sq.m
Frontage	30.5m
GFA	6,500 sq.m
FSR	5.0
Height	8 storeys (25.5m)

Site 2B (Interior Lot)

Site Area	810 sq.m
Frontage	19.0m
GFA	3,925 sq.m
FSR	4.8
Height	5 storeys (16.5m)

4.3 mixed-use shoulder



Mixed-Use Shoulder areas correspond to lands where historically or presently the commercial influence of Broadway Avenue has extended into adjacent areas. With the exception of the southeast and southwest corners of Broadway Avenue and 8th Street East, these lands are currently subject to the B5 zoning district regulations that are also applicable to the Mixed-Use Corridor area. However, the existing character of these areas is significantly different and to a certain degree so should the planned context.





The Mixed-Use Shoulder areas currently comprise of a mix and variety of uses and building forms, but mainly low-rise (1 to 2-storey) commercial or former industrial buildings as well as some house forms. With the exception of some heritage significant structures, most buildings are generally of poor or insignificant architectural quality. Very little retail exists and there is a lack of consistency with respect to how the buildings interface with the street and each other. In sum, these areas currently have a weak urban identity in terms of function, look and feel.

The future role and function of the Mixed-Use Shoulder areas should be to serve as a transition between the more intense commercial function of Broadway Avenue and the adjacent residential areas. In addition to providing significant opportunities for residential, commercial and/or office intensification, the Mixed-Use Shoulder areas are ideal locations for retail and services that mainly serve the local residential population. Relative to Broadway Avenue, these locations can provide for more affordable commercial or office spaces for uses such as an artist studio, grocer, drug store, dentist or hairdresser. Although a mix of uses including at-grade retail can exist, they should do so in a built environment that is more 'village' in character. This primarily translates into a greener streetscape and broader setbacks than on Broadway Avenue, transitioning to the residential areas.



Mixed Use Shoulder areas provide a transition in scale, intensity and use from Broadway Avenue to the adjacent and surrounding residential neighbourhoods

Recommended Development Standards

The following are the recommended development standards for Mixed-Use Shoulder areas. Where necessary, they are accompanied by supporting contextual commentary and rationale in coloured italics. Where an alternative standard is not specified in this or other sections of the Plan, existing developments standards can be assumed.

Uses

- Except for lands south of 8th Street E., a broad mix of uses should continue to be permitted, except for automotive related uses such as services stations, drive-throughs and other uses that detract from the quality of the streetscape and pedestrian-oriented environment.
- For lands south of 8th Street E., commercial uses should be restricted to professional offices and other complimentary uses to adjacent residential areas. Retail uses that generate significant noise or traffic should be discouraged.
- In addition to retail uses such as shops and restaurants, residential and office uses should be permitted at-grade, however they should provide positive frontages (entries and windows) to the street. At-grade residential units should be accessed from the street.

Even multiple storey residential buildings should provide at-grade, street-accessed units if other street-related uses are not proposed. This ensures an animated and safer streetscape.

- Where they can be accommodated, pedestrian-oriented larger format retail uses should be permitted.

Base Building refers to a building or the component of a building that occupies the first three storeys and that frames the street. It is what forms the 'street wall' and corresponds to what is defined as the 'Historic Tier' in the Nutana Local Area Plan.

Base Building

- To be consistent with the character of existing mixed-use heritage buildings, grade level heights should be no less than 4.5 metres for commercial uses and 4.0 meters for residential uses.
- To reinforce a consistent and well-defined street edge of a pedestrian scale, the Base Building should be a minimum of 7.5 metres (2-storeys) and maximum of 12.5 metres (3-storeys) in height.
- To ensure a consistent street wall that is more 'village' in character, a 'build-within zone' should be established of 2.0 to 4.0 metres. A minimum 3.0 metre setback with grade shifts such as a porch should be provided where at-grade residential uses are proposed.

Within these setbacks additional landscaping or broadened sidewalks can be provided depending on the at-grade uses. In the case of at-grade residential uses, a setback of 3.0 metres along with several steps ensure adequate space for privacy and/or landscaped screening.



- Interior side yard setbacks are not required and will depend on at-grade uses. Where a publicly accessible side yard setbacks is provided it should be a minimum of 3.0 metres and have positive or active frontages such as windows and entries to enhance safety.
- Rear yard setbacks should not be required for non-residential uses, however where a Base Building is abutting a property within a Neighbourhood area, it may be subject to above-grade setbacks in accordance with the Angular Plane Guidelines.
- Where at-grade commercial uses fronting the rear lane are proposed and permitted, a minimum 3.0 metre rear yard setback should be provided.
- Where residential uses are provided at-grade with entries and windows facing the rear yard, a minimum 7.5 metre setback should be provided for adequate amenity space and privacy.
- At-grade parking should be prohibited from any street frontages and directed to the rear of the Base Building, accessed through the rear lanes.
- While below-grade parking should be encouraged, above-grade parking should be permitted within the Base Building under the following conditions:
 - Above-grade parking areas are included in the density calculation.
 - At-grade uses are provided on all street frontages with no less than 15.0 metres depths from principle streets and no less than 7.5 metres depths from side street
 - The façade treatment of the above-grade parking is subject to the Design Guidelines.
 - Access to parking is provided from the rear lane

Height & Massing

- To ensure a proportional relationship to the street, adequate transitions to adjacent low-rise areas and good urban design at an appropriate scale for the Nutana context, buildings should not exceed 30.0 metres (7 to 9-storeys depending on the uses) in height.

Although appropriate for complementary mixed uses, the lands south of 8th Street East should serve as a transition in uses of uses as well as scale.

See Mixed-Use Corridor for additional commentary.

- To ensure a street wall height that is consistent with heritage character of the Broadway area, building components above the Base Building should provide a minimum 3.0 metres setback from the face of the building that is the street wall. Modest exceptions may be permitted on Key Corner Sites.

See Mixed-Use Corridor for additional commentary.

- To minimize visible above-grade blank wall conditions and to provide for adequate separation distances between taller buildings, a minimum 6.0 metre rear and interior yard step back should be required for all buildings taller than 5-storeys.

See Mixed-Use Corridor for additional commentary.

These standards address the components above the Base Building where developments taller than 3-storeys are proposed. These standards address what is defined as the High Density Tier in the Nutana Local Area Plan.

- A minimum 18.0 metre separation distance between the principle faces of residential buildings taller than 5-storeys should be provided to ensure adequate light and privacy.
- To ensure appropriate transitions to, and interfacing with, adjacent properties within Neighbourhood areas, the massing of buildings should be subject to the Angular Planes Guidelines.

See Mixed-Use Corridor for additional commentary.

- Where the principle frontage of a development is on a street with a width less than 30.0 metres, the upper storeys should provide additional step backs subject to the Angular Planes Guidelines.

Building heights that exceed the width of the street can be mitigated through the stepping back of the top storeys.

- To minimize excessively massed buildings, floor plate dimensions for levels above the 5th storey should not exceed 35.0 metres.
- To ensure buildings that are consistent with these standards and of good and viable form, the following minimum frontage standards should be applied:
 - Frontage of less than 15.0 metres = 3-storey maximum height
 - Frontage between 15.0 metres and 30.0 metres = 5-storey maximum height
 - Frontages of 30.0 metres or greater = 9-storey maximum

See Mixed-Use Corridor for additional commentary.

Gross Floor Space Ratio (FSR) is a measure of the total amount of building floor area relative to the area of the property. Although density is not an adequate mechanism for defining the shape and form of development, it is an appropriate means for controlling the overall scale of a building relative to its site.

Density standards should be tied to contexts rather than property frontage. Being a ratio that directly corresponds to the scale of the property, it is unnecessary to differ the ratio from property to property within a context where more uniformity in building forms is desired.

Development Density

- To ensure appropriately scaled developments for the Broadway area context, the gross floor space ratio should not exceed 5:1.

See Mixed-Use Corridor for additional commentary.

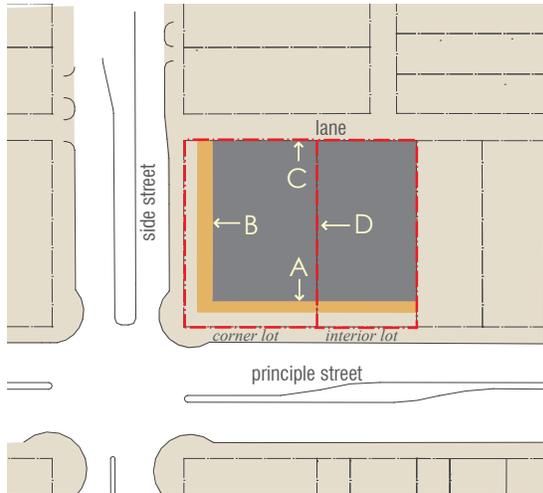
- To provide an incentive for certain public benefits such as heritage retention, affordable housing, public parking, contributions to public realm improvements or other priorities determined by the community, a density bonus should be considered for sites where it can be accommodated and where all other development standards are complied with, including heights and angular planes.

See Mixed-Use Corridor for additional commentary.



Summary of Key Recommended Development Standards

base building placement

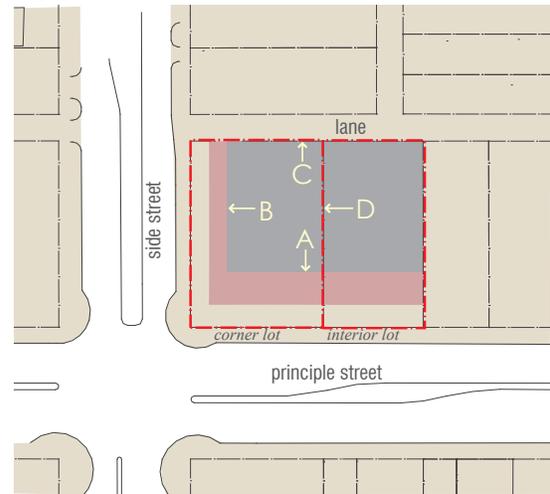


Buildings should be placed within the shaded area as shown.

- A Front Setback: 2.0m-4.0m
- B Side Street Setback: 2.0m-4.0m
- C Rear Setback: Not required (Minimum 7.5m for at-grade residential uses)
- D Side Yard Setback: Not required

Build within zone: Allows for widened sidewalks for pedestrian amenity and spill-out activities such as sidewalk cafes.

parking



Vehicular access permitted only from rear lane. At-grade parking (enclosed and unenclosed) should be placed in the shaded area shown.

- A Front Setback: 15.0m minimum
- B Side Street Setback: 7.5m minimum
- C Rear Setback: Not required
- D Side Yard Setback: Not required

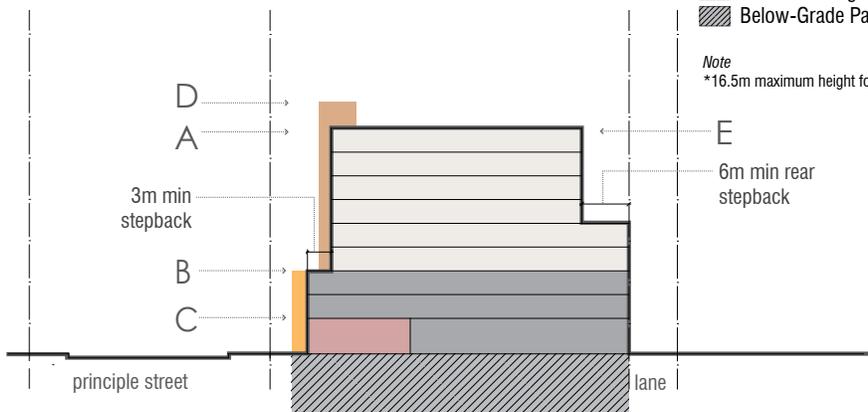
Required at-grade uses

building profile

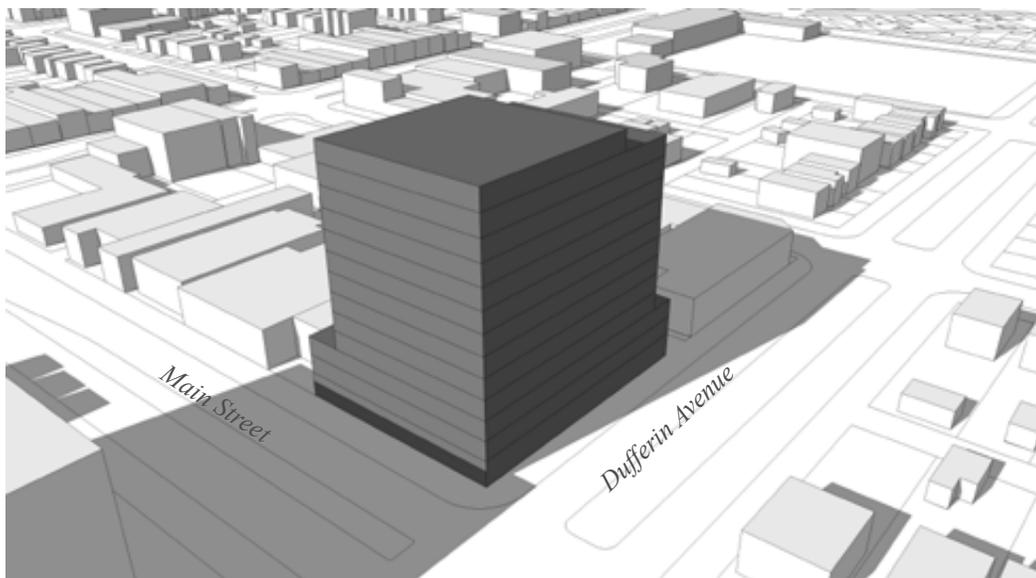
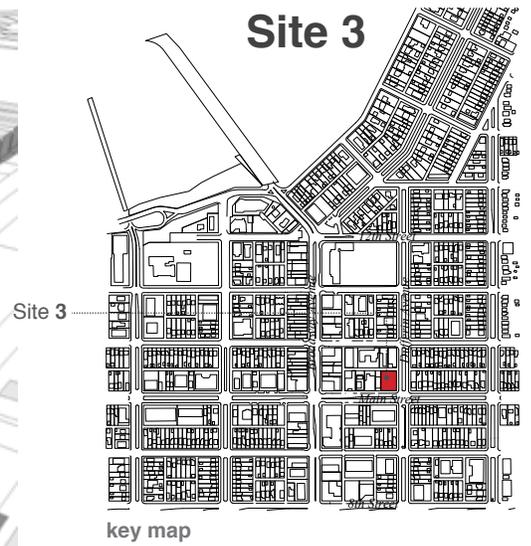
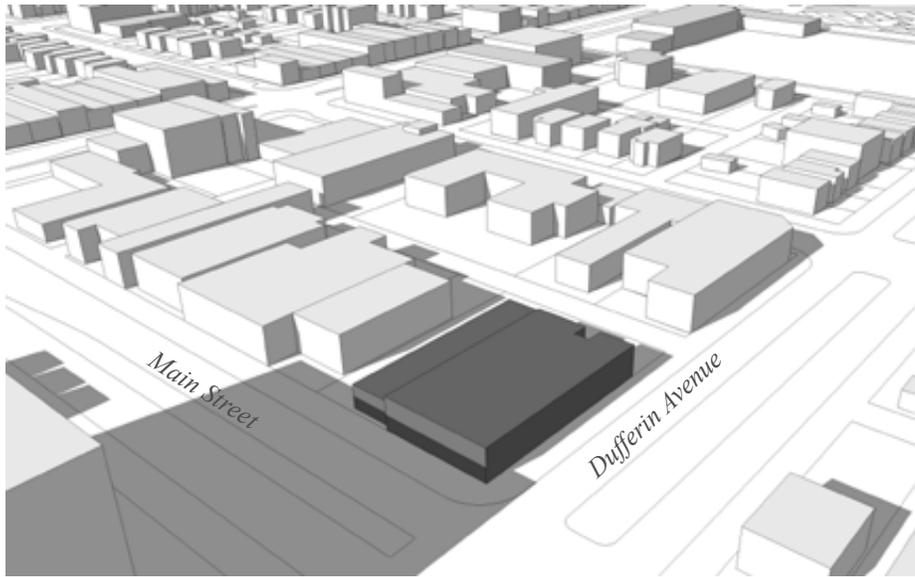
- A Maximum height: 30.0m*
- B Streetwall: Minimum 7.5m to Maximum 12.5m
- C Minimum grade-level height: 4.5m for Commercial/4.0m for Residential
- D Key Corner Sites: Up to 3.0m allowed above maximum height, maximum 5.0m frontage on either street face
- E Massing may be subject to angular plane

- Required At-grade uses
- Allowable Building Encroachment for Key Corner Sites
- Build within zone
- Allowable At-grade and Above-grade parking
- Below-Grade Parking Zone

Note
*16.5m maximum height for lands south of 8th Street East



Demonstration of Recommended Development Standards



Current Zoning Standards

Site Area	1,430 sq.m
Frontage	33.5m
GFA	14,300 sq.m
FSR	10.0
Height	13 storeys
(40.0m)	



Proposed Standards

Site Area	1,430 sq.m
Frontage	33.5m
GFA	5,660 sq.m
FSR	4.0
Height	7 storeys (22.5m)

development
framework



4.4 urbanneighbourhood



Urban Neighbourhood areas correspond to lands with existing or potential high-density residential forms. These lands straddle the mixed-use and low-density areas and are generally subject to a variety of zoning districts, but mostly RM5. The existing built character varies greatly from house forms to mid-rise ‘tower-in-park’ apartments.

Most of these lands are built-up and not anticipated to change in any significant way. However there are a number of vacant and underutilized properties that can accommodate further residential intensification for which guidance ought to be provided. In conjunction with the mixed-use areas, the future role and function of Urban Neighbourhood areas should be to ensure that a diversity of housing choices exist in the Nutana community and at densities appropriate for their location directly adjacent to shops, services and public transit. Unlike the mixed-use areas however, Urban Neighbourhoods are predominantly residential in character, providing all the charms and tranquillity of the typical Nutana neighbourhood except at higher densities.





Urban Neighbourhoods correspond to lands with existing or potential higher density residential forms

Recommended Development Standards

The following are the recommended development standards for Urban Neighbourhood areas. Where necessary, they are accompanied by supporting contextual commentary and rationale in coloured italics. Where an alternative standard is not specified in this or other sections of the Plan, existing developments standards can be assumed.

Uses

- Residential uses should be permitted in a variety of building typologies, including row houses, stacked townhouses, garden or courtyard apartments and mid-rise apartments.
- Work-live units and small-scale convenience retail or services should be permitted at-grade.

Although primarily residential, compatible commercial uses or services that do not generate high traffic volumes and that serve the local residents are appropriate for high-density areas. Work-live units provide for affordable opportunities for entrepreneurs and artists to remain or establish themselves in the community and within close proximity to Broadway Avenue.

Base Building

- To be consistent with the historic residential character of the Nutana neighbourhoods, street-access units should be provided at-grade with grade level heights no less than 4.0 meters.
- To reinforce a consistent and well-defined street edge of a residential scale, the Base Building should be a minimum of 7.0 metres (2-storeys) and maximum of 10.0 metres (3-storeys) in height.

Base Building refers to a building or the component of a building that occupies the first three storeys and that frames the street.

- To ensure a consistent street wall that is residential in character, a 'build-within zone' should be established of 3.0 to 6.0 metres. Within this zone, a landscaped front yard with privacy screening and/or grade shifts that incorporate a porch should be provided.
- Interior side yard setbacks are not required. Where a publicly accessible side yard setbacks is provided it should be a minimum of 3.0 metres and have positive or active frontages such as windows and entries to enhance safety.
- Where residential uses are provided at-grade with entries and windows facing the rear yard, a minimum 7.5 metre setback should be provided for adequate amenity space and privacy.
- At-grade parking should be prohibited from any street frontages and directed to the rear of the Base Building, accessed through the service lanes.
- While below-grade parking should be encouraged, above-grade parking should be permitted within the Base Building under the following conditions:
 - Above-grade parking areas are included in the density calculation.
 - At-grade uses are provided on all street frontages with no less than 15.0 metres depths from principle streets and no less than 7.5 metres depths from side street
 - The façade treatment of the above-grade parking is subject to the Design Guidelines.
 - Access to parking is provided from the rear lane

These standards address the components above the Base Building where developments taller than 3-storeys are proposed.

Height & Massing

- To ensure a proportional relationship to the street, adequate transitions to adjacent low-rise areas and good urban design at an appropriate scale for the Nutana context, buildings should not exceed 30.0 metres (9-storeys) in height.

See Mixed-Use Corridor for additional commentary.

- To ensure a street wall height that is consistent with the historic residential character of the Broadway area, building components above the Base Building should provide a minimum 3.0 metres stepback from the face of the building that is the street wall.

In addition to emphasizing a low-rise street wall, the step back helps to mitigate wind impacts of taller buildings.

- To provide for adequate separation distances between taller buildings, a minimum 6.0 metre interior and rear yard step back should be required for components above the Base Building.
- A minimum 18.0 metre separation distance between the principle faces of residential buildings taller than 5-storeys should be provided to ensure adequate light and privacy.



- To ensure appropriate transitions to, and interfacing with, adjacent properties within Neighbourhood areas, the massing of buildings should be subject to the Angular Planes Guidelines.

See Mixed-Use Corridor for additional commentary.

- Where the principle frontage of a development is on a street with a width less than 30.0 metres, the upper storeys should provide additional step backs subject to the Angular Planes Guidelines.

Building heights that exceed the width of the street can be mitigated through the stepping back of the top storeys.

- To minimize excessively massed buildings, floor plate dimensions for levels above the Base Building should not exceed 35.0 metres.
- To ensure buildings that are consistent with these standards and of good and viable form, the following minimum frontage standards should be applied:
 - Frontage of less than 15.0 metres = 3-storey maximum height
 - Frontage between 15.0 metres and 30.0 metres = 5-storey maximum height
 - Frontages of 30.0 metres or greater = 9-storey maximum

See Mixed-Use Corridor for additional commentary.

Development Density

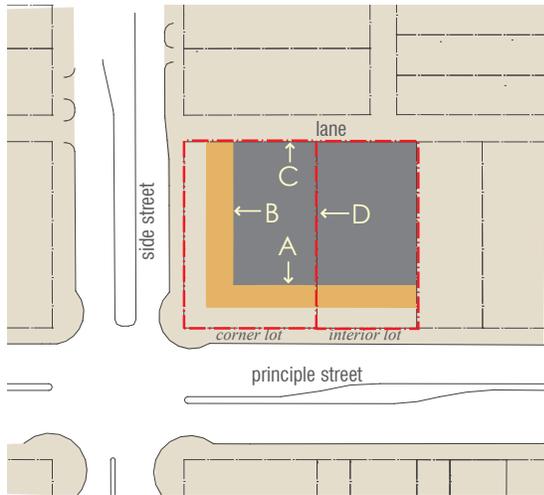
- To ensure appropriately scaled developments for the Broadway area context while providing for adequate separation distances and grade-level amenity areas, the gross floor space ratio should not exceed 4:1.

See Mixed-Use Corridor for additional commentary.

Gross Floor Space Ratio (FSR) is a measure of the total amount of building floor area relative to the area of the property. Although density is not an adequate mechanism for defining the shape and form of development, it is an appropriate means for controlling the overall scale of a building relative to its site.

Summary of Key Recommended Development Standards

base building placement

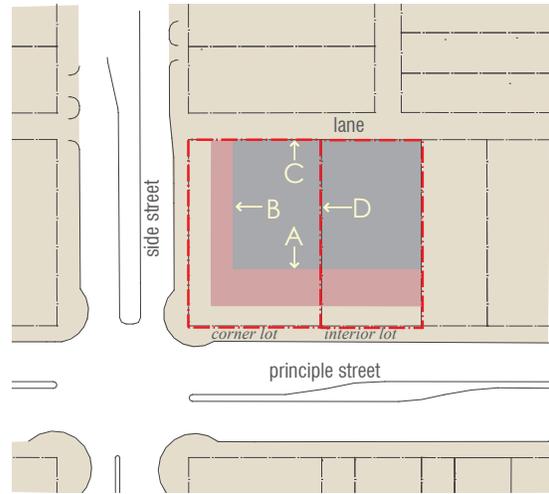


Buildings should be placed within the shaded area as shown.

- A Front Setback: 3.0m-6.0m
- B Side Street Setback: 3.0m-6.0m
- C Rear Setback: Not required (7.5m minimum for at-grade residential uses)
- D Side Yard Setback: Not required

Build within zone: Allows for a landscaped front yard with privacy screening and/or grade shifts that incorporate a porch

parking

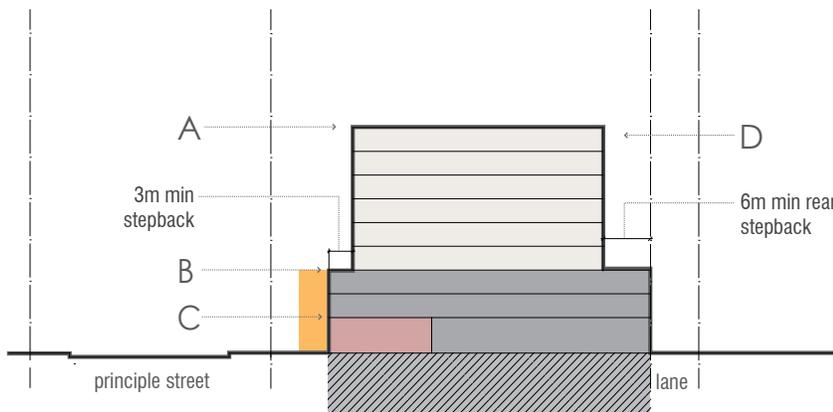


Vehicular access permitted only from rear lane. At-grade parking (enclosed and unenclosed) should be placed in the shaded area as shown.

- A Front Setback: 15.0m minimum
- B Side Street Setback: 7.5.m minimum
- C Rear Setback: Not required
- D Side Yard Setback: Not required

Required at-grade uses

building profile

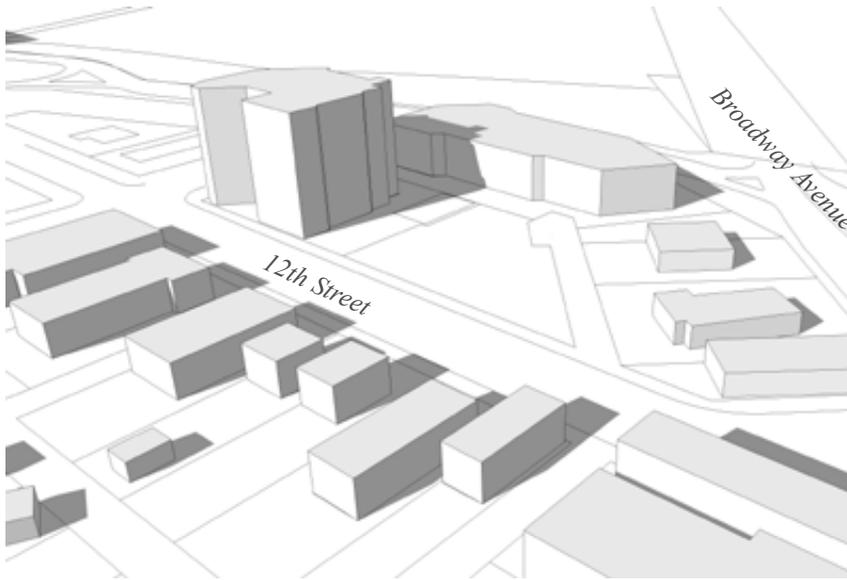


- A Maximum height: 30.0m
- B Streetwall: Minimum 7.5m to Maximum 10.0m
- C Minimum grade-level height: 4.0m
- D Massing may be subject to angular plane

- Required Street-Accessed Units
- Build within zone
- Allowable At-grade and Above-grade parking
- Below-Grade Parking Zone



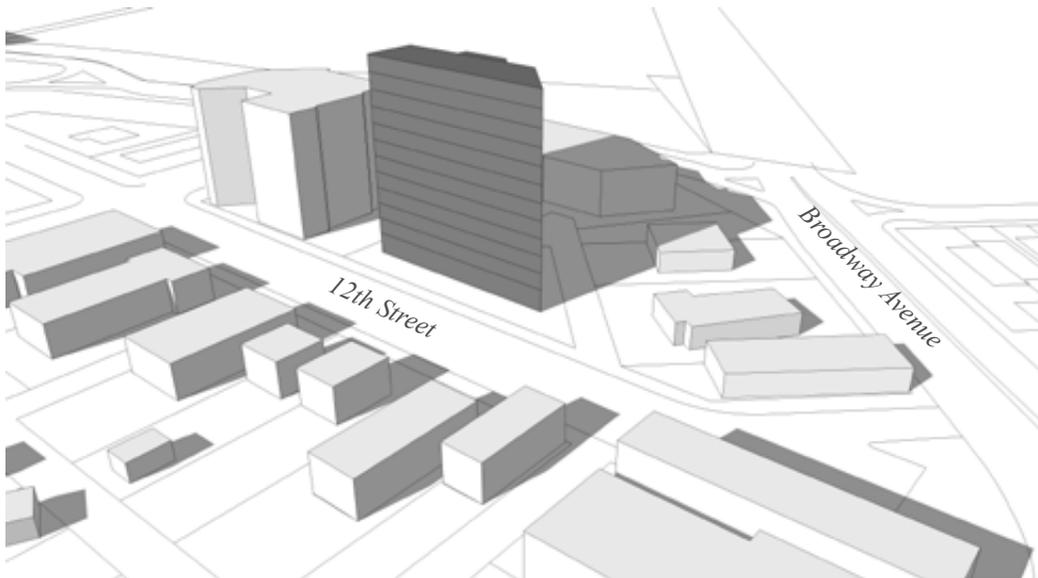
Demonstration of Recommended Development Standards



Site 4



key map



Current Zoning Standards

Site Area	1,266 sq.m
Frontage	54.0m
GFA	6,330 sq.m
FSR	5.0
Height	12 storeys
	(37.0m)



Proposed Standards

Site Area	1,266 sq.m
Frontage	54.0m
GFA	5,000 sq.m
FSR	4.0
Height	9 storeys (28.0m)

4.5 neighbourhood/ neighbourhood corridor



Neighbourhood Corridors



Neighbourhoods

Neighbourhood and Neighbourhood Corridor areas are comprised of predominantly built-up and stable residential areas. While primarily low-density house forms characterize Neighbourhood areas, Neighbourhood Corridors generally correspond to important streets lined with mostly low-rise apartment buildings. This integration of housing variety in close proximity is common to traditional neighbourhoods and a treasured aspect of the Nutana community.





Each of these areas serves a vital role and function in providing a variety of housing choices to suite a diversity of needs and lifestyles. Furthermore, it is in these areas where the rich and extraordinary history of Nutana is most evident, as revealed in the remarkably well-preserved historic homes and towering elm-lined streets. It is these very qualities, combined with its civic landmarks and vibrant town centre, that are the hallmarks of a livable and complete community and what makes Nutana so appealing.

Although the Local Area Plan and this study do not intend for significant change here, modest infill and redevelopment within Neighbourhood and Neighbourhood Corridor areas has historically occurred and pressures will likely continue in the future. Accordingly, ensuring the stability of these areas and the retention of their respective built characteristics should be a fundamental objective when any alterations or developments are proposed.

A key recommendation of this study is for the preparation of infill and architectural guidelines that will provide a necessary resource for applicants when preparing plans and for the City in assessing applications. At a minimum, these guidelines should provide a detailed assessment of historic and existing conditions, an inventory of architectural styles, and standards for the placement, scale, and design of new buildings or additions. Guidance could also include colour, material, roof, detailing and landscaping palettes. To ensure the protection of existing heritage resources and sympathetic infill, a Heritage Conservation District may be warranted.



Neighbourhood and Neighbourhood Corridor Character Areas are comprised of predominantly built-up and stable residential areas

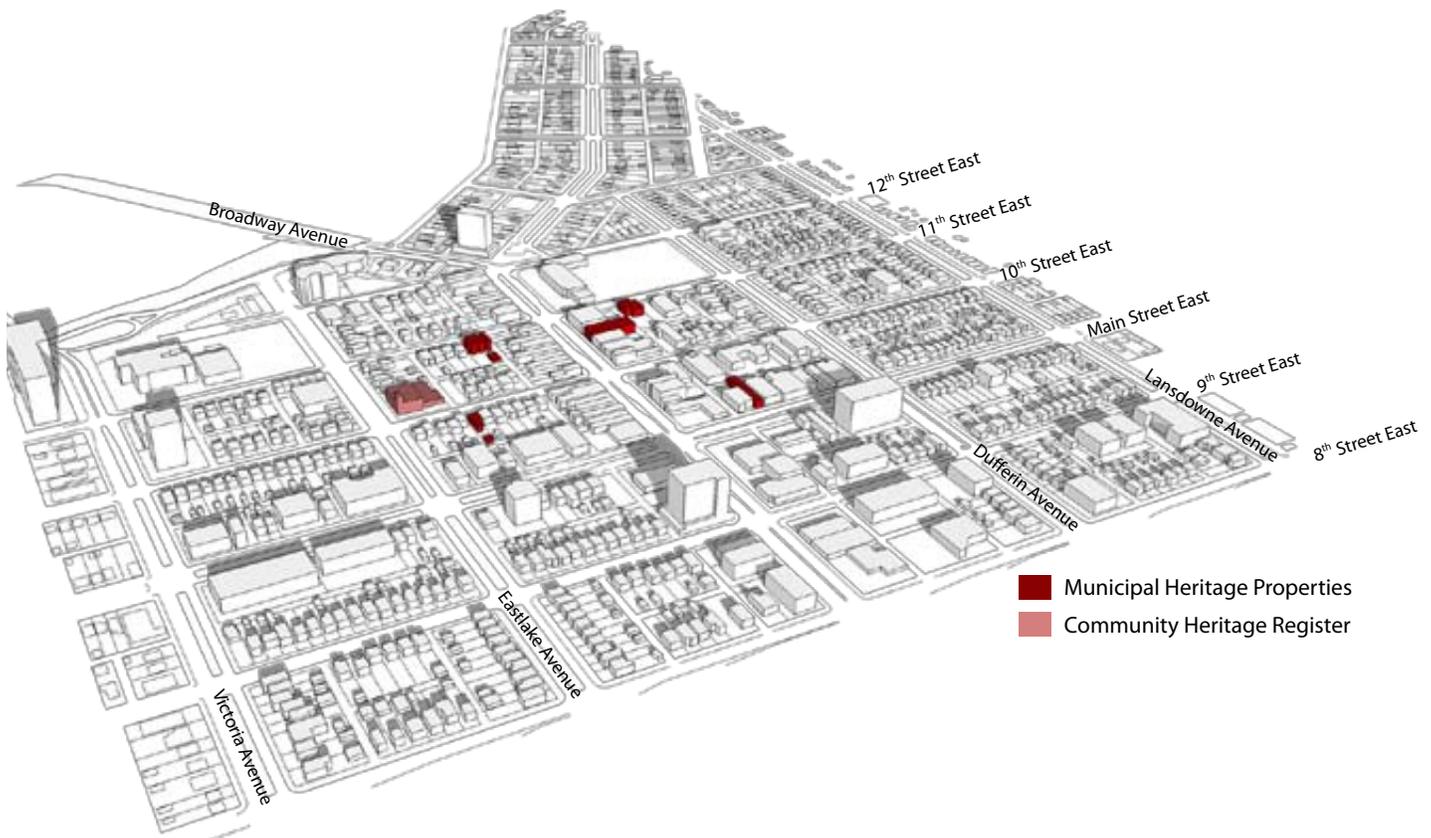
4.6 heritage resources

Despite perceptions to the contrary, Broadway Avenue is not a contiguous heritage environment. Although there are a number of intact historic buildings concentrated in the Heritage Core character area that lend to its charm, they alone do not define Broadway's distinct character. However, every effort possible should be made to retain and restore the heritage resources that still exist as they hold great symbolic importance to the entire city and they lend to the novelty and distinction of Broadway Avenue.

Heritage Resources

Heritage Resources refers to existing municipally identified properties that generally include late 19th and early 20th century buildings and structures. These buildings are recognized for their historic or architectural significance and are categorized as follows:

Municipal Heritage Properties - existing designated properties under the Heritage Property Act which should be protected to the greatest extent possible and where abutting developments ought to provide appropriate transitions in scale and complementary design and material qualities.





Community Heritage Register - existing properties that have been listed on the City of Saskatoon’s heritage register, for which development and building applications are subject to review by the Development Services Branch in the Community Services Department. Properties on the Community Heritage Register provides an opportunity to recognize the heritage significance of a property, where the property must be maintained and key exterior heritage features cannot be altered for a fixed number of years. Unlike designated municipal heritage properties, there is no long-term guarantee of protection for the property.

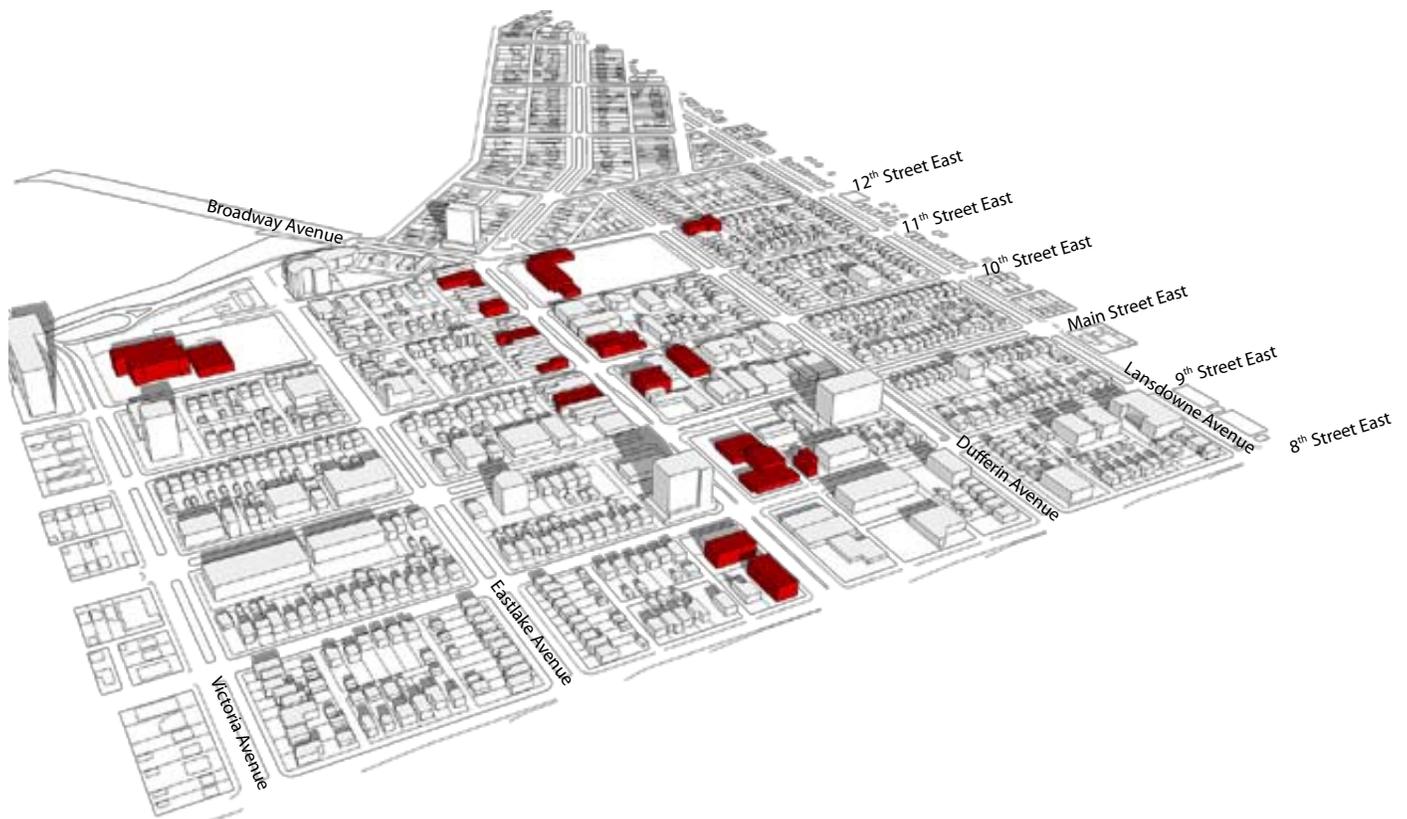
When considered for intervention or adjacent development is proposed, an assessment should be undertaken to determine what, if any, aspects should be protected or respected. Where heritage significance is confirmed, it is recommended that these aspects be designated.

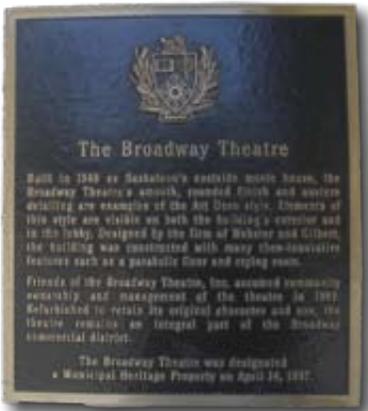


Properties of Interest

As is common in many other comparable areas across North America that are not subject to a Heritage Conservation District, municipally identified heritage resource do not necessarily reflect a complete accounting of all properties of heritage significance. Through the course of this study process and review of background materials, a number of other potential heritage resources within the mixed-use areas were identified as Properties of Interest.

As with the properties on the Community Heritage Register, when considered for intervention, redevelopment or where adjacent development is proposed, an assessment should be undertaken to determine what, if any, aspects should be protected or respected. Where heritage significance is confirmed, it is recommended that these aspects be designated.





Recommended Development Standards

Where development is proposed on or adjacent to a designated heritage property, the following standards are recommended:

- To determine aspects to protect and mitigation measures, a heritage impact study should be a requirement of any proposal on or adjacent to Heritage Resources or Properties of Interest. A heritage expert will undertake an assessment to ensure mitigating measures are considered.
- Where an entire structure is designated it should be retained and restored
- Where a façade is designated, the Base Building height should be consistent with the height of the façade to ensure a seamless integration
- The Base Building and taller elements of a proposed development should visibly step down in height to adjacent designated heritage properties
- Where appropriate, a density bonus should be considered for proposals that protect and restore heritage properties in conjunction with new development
- Heritage properties should not be subject to any parking requirements
- Development on or adjacent to designated properties should be subject to the Design Guidelines provided in this report





4.7 public/civic landmark

A distinctive characteristic of Broadway Avenue is the prominence of major civic uses, most of which are of heritage significance. The presence of schools and places of worship introduce variety to the streetscape, provide amenity spaces, and lend to the diversity of uses and people in the area. These uses should continue to exist into the future and every effort possible should be made to physically enhance their landmark qualities through restoration, facade improvements and complementary landscaping.

Where additions or external alterations are proposed, the design should be sympathetic and subordinate to the heritage aspects of the buildings while clearly distinguishing between that which is new and old. To enhance their contribution to the vibrancy and character of Broadway Avenue, publicly accessible and visible areas such as surface parking and landscaped setbacks should be designed to contribute positively to the streetscape and where possible should accommodate a multiplicity of purposes. For example, surface-parking areas can be specially paved to double as plazas and gather spaces for special events.



There is a strong civic presence in the Broadway Area with landmarks such as (from left) Nutana Collegiate, Oskayak High School and St. Joseph's Roman Catholic Church



4.8 retail frontages

A defining characteristic of Broadway Avenue is its fine-grained rhythm of shops and restaurants. The success of a retail street depends greatly on the continuity and intensity of complementary uses and on the quality of the pedestrian environment. Although by all accounts Broadway Avenue is a successful retail environment, it can be improved upon, as there remains a number of significant 'gaps' as a result of surface parking lots, blank walls and non-retail uses at-grade.

A key objective of this Plan is to strengthen and reinforce the retail environment to ensure Broadway's continued success as both a local and regional draw. To this end, the Development Framework identifies a hierarchy for coordinating and directing retail uses:



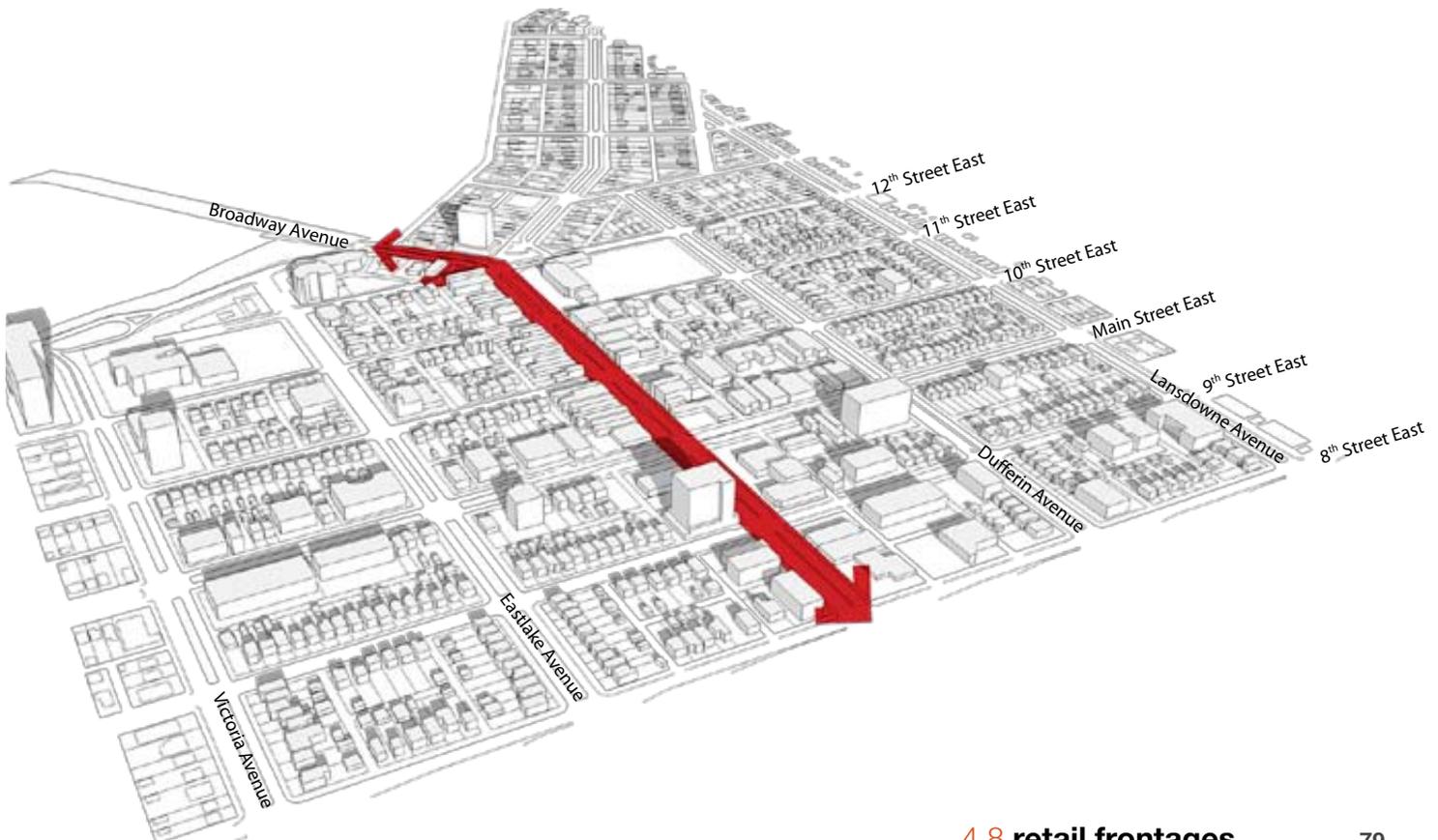
Primary Retail Frontage

Primary Retail Frontages correspond to properties that front onto Broadway Avenue. At-grade retail uses that address the sidewalk should be a requirement for these frontages to ensure a continuous active streetscape. The character and design of the storefronts in these locations is also of great importance to the atmosphere and character retention of the streetscape. Specifically, the recommended standards include:

- An articulation of narrow storefronts in the range of 7.5 metres.
- Limiting the width of at-grade retail units at the street edge to no greater than 15.0 metres.
- Shop fronts should be designed with a high level of transparency comprised of generally no less than 75% glazing.
- Where larger retail formats are proposed they should be directed to second levels.
- Second storey and/or double storey commercial uses are encouraged to intensify the retail presence and activity while providing for alternative and affordable retail space.
- Spill out activity such as sidewalk cafés are highly encouraged where they can be accommodated.
- Weather protection for pedestrians is encouraged through the use of awnings and canopies.
- Commercial signage should be of high quality and should add diversity and interest to the streetscape.



Typical storefronts along Broadway range from 7.5m to 15m in width



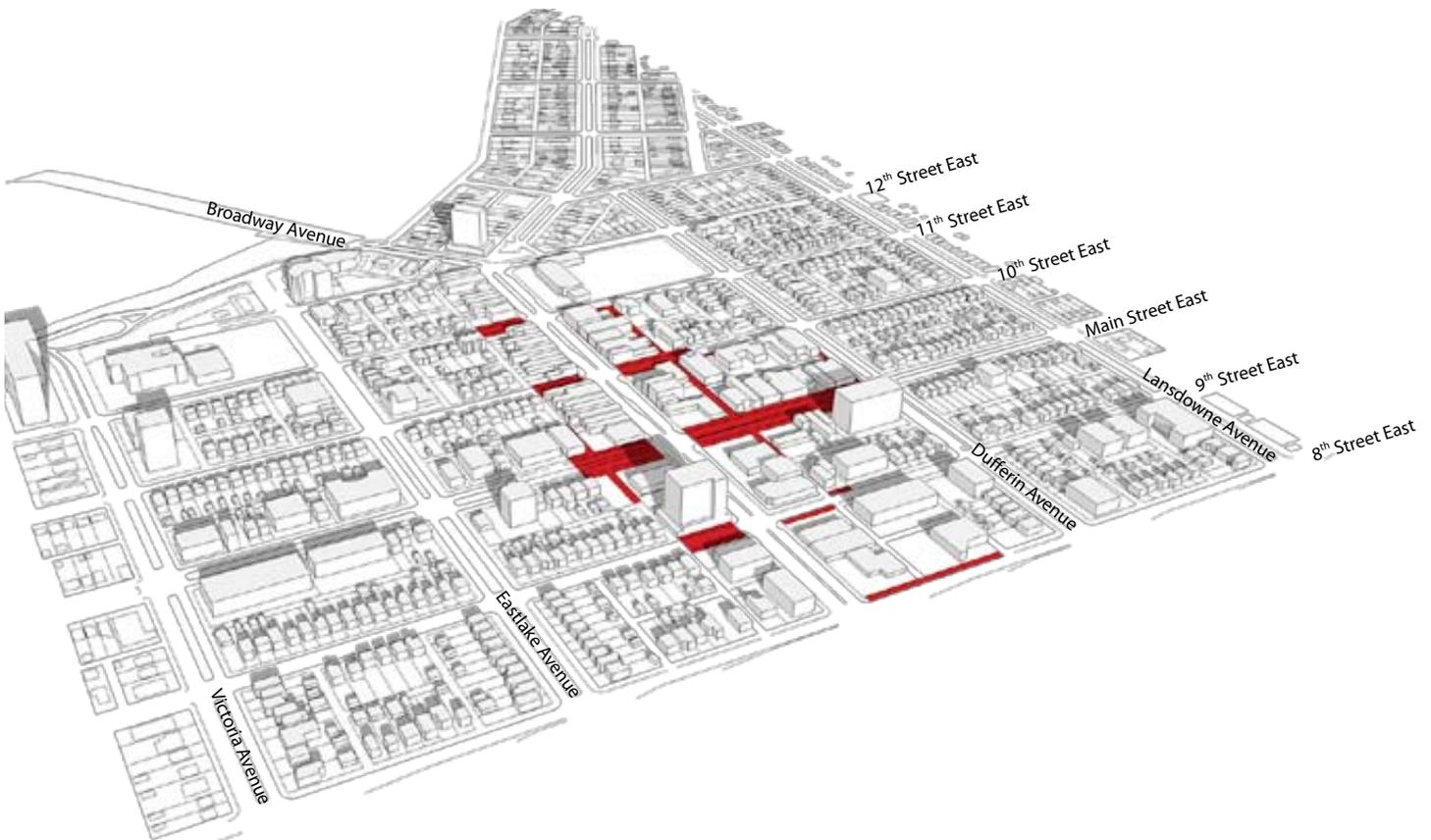
Secondary Retail Frontages

Secondary Retail Frontages corresponds to other locations where street-related retail uses are permitted but not required. This includes the side streets off Broadway Avenue, the Mixed-Use Shoulder areas and rear lanes that are not shared by residential uses. These frontages provide opportunities for a wide variety of potential commercial uses including ones that may be less suited or not viable for Broadway Avenue. Although the design quality in these locations is as important, greater variations in scale and the character of the street interface should be permitted, including:

- A range of storefront widths
- Live-work spaces and studios that can also serve as showrooms or retail outlets
- Larger format commercial uses such as a grocery store or drug store
- Setbacks that may vary depending on the street and context



An example of existing secondary retail frontages along a side street





4.9 key corner sites

Key corner sites occupy important intersections of streets and gateways into the Broadway Avenue commercial area. Articulating key corners through the massing and architectural design of buildings can enhance the civic quality and image of the area and serves to orient visitors. To enhance the visual prominence of key corners, the design of buildings on these sites are subject to the following standards:



- To enhance the distinction and landmark quality of new buildings on key corner sites, a 1.5 metre encroachment into the step back above the Building Base should be permitted at the corner of the building for up to 5.0 metres of frontage on either street face.
- An exception to the maximum height of no greater than 3.0 metres should be permitted for the corner massing treatment.
- Distinctive architectural treatments are encouraged and can include vertical slender elements such as drums, spires and turrets.
- New developments on key corner sites should orient to both street frontages with respect to storefronts and entries.



An example of a key corner site treatment for a building



4.10 angular planes guidelines

Building heights of up to 30.0 metres are recommended for Mixed Use Corridor, Mixed Use Shoulder and Urban Neighbourhood areas. In conjunction with height controls are recommendations for massing controls that include the use of angular planes. Angular planes are commonly used to define appropriate transitions to adjacent low-rise areas and to ensure that building do not overwhelm the streets they frame. Specifically, the angular plane is intended to define the extents of the development envelope to guide above-grade building step backs. New developments that are subject to the angular plane guidelines should be massed with good form within these envelopes.

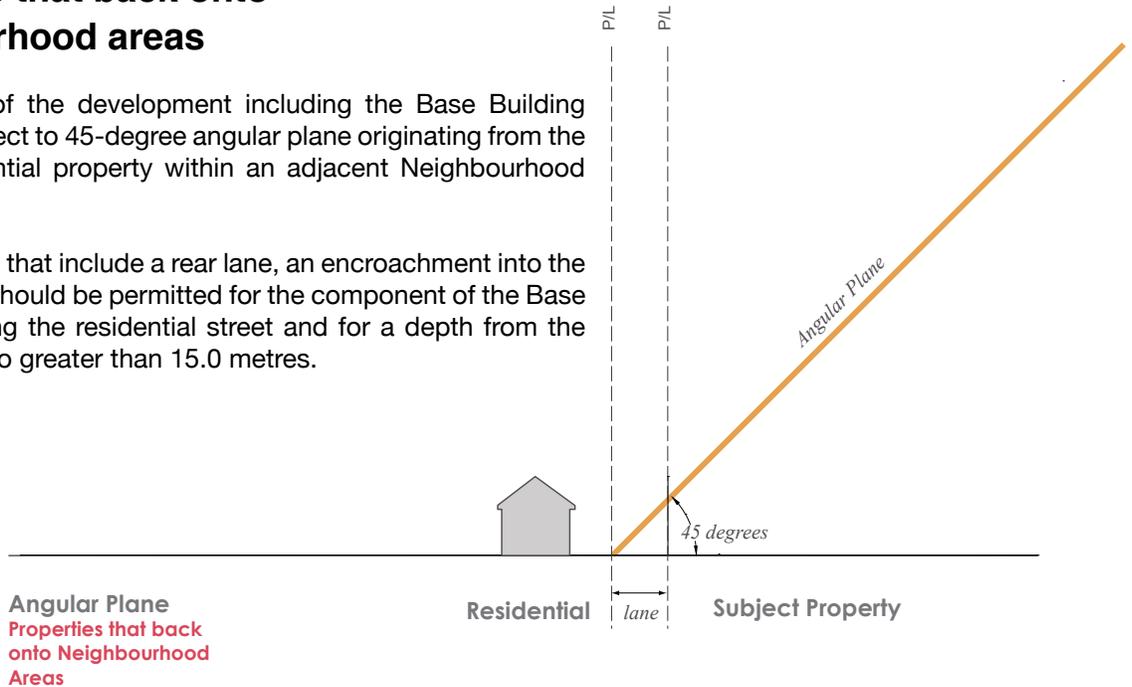


Properties in Mixed Use Corridor, Mixed Use Shoulder and Urban Neighbourhood areas are subject to the following angular planes:

1 Properties that back onto Neighbourhood areas

The massing of the development including the Base Building should be subject to 45-degree angular plane originating from the nearest residential property within an adjacent Neighbourhood area.

On corner sites that include a rear lane, an encroachment into the angular plane should be permitted for the component of the Base Building fronting the residential street and for a depth from the side street of no greater than 15.0 metres.



2 Properties that face Neighbourhood areas

Where a development fronts onto a street facing a Neighbourhood area, the massing should be subject to 45-degree angular plane originating from the top of the Base Building.



FIGURE 4: ANGULAR PLANES

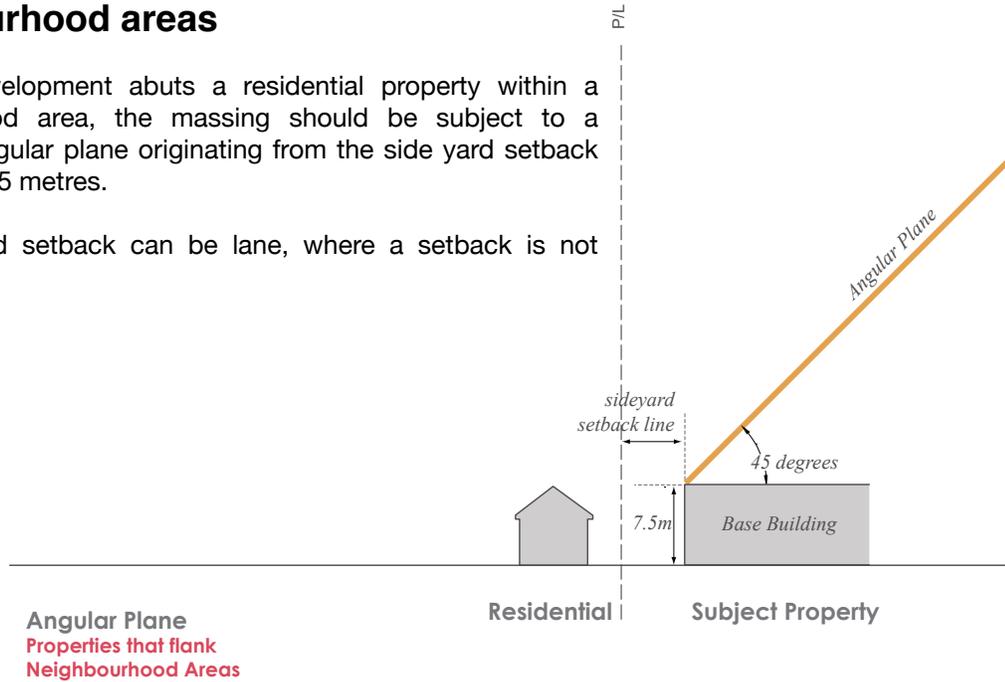




3 Properties that flank Neighbourhood areas

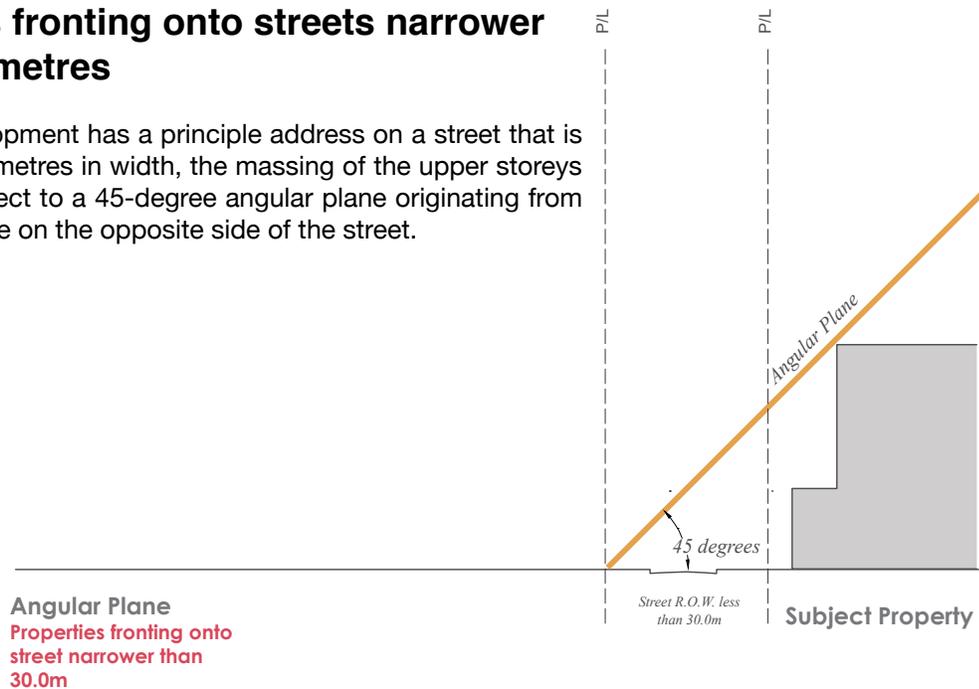
Where a development abuts a residential property within a Neighbourhood area, the massing should be subject to a 45-degree angular plane originating from the side yard setback at height of 7.5 metres.

The side yard setback can be lane, where a setback is not required.



4 Properties fronting onto streets narrower than 30.0 metres

Where a development has a principle address on a street that is less than 30.0 metres in width, the massing of the upper storeys should be subject to a 45-degree angular plane originating from the property line on the opposite side of the street.





4.11 design guidelines

The purpose of these Design Guidelines is to provide appropriate design guidance for new development in the Mixed-Use Corridor, Mixed-Use Shoulder and Urban Neighbourhood areas. These guidelines are informed by and reinforce the objectives and standards of the **Development and Public Realm Frameworks**. Although they seek to provide clarity on intended outcomes of the Plan, they also provide flexibility within certain parameters to encourage distinction, variety and creative architectural responses.

The Design Guidelines address of the following building design features:

- Building Expression
- Orientation and Placement
- Street Wall
- Heritage Contexts
- Corner Sites
- Storefronts
- Street Access Units
- Roof Treatment
- Above-Grade Parking
- Material & Architectural Quality
- Sidewalk Cafés
- Building Lighting
- Signage
- Sustainable Design

The Design Guidelines are informed by best practices, tailored to the unique Broadway area context. The guidelines help to reinforce the objectives of the Broadway 360° Development Plan, while providing an added layer of guidance and clarity.



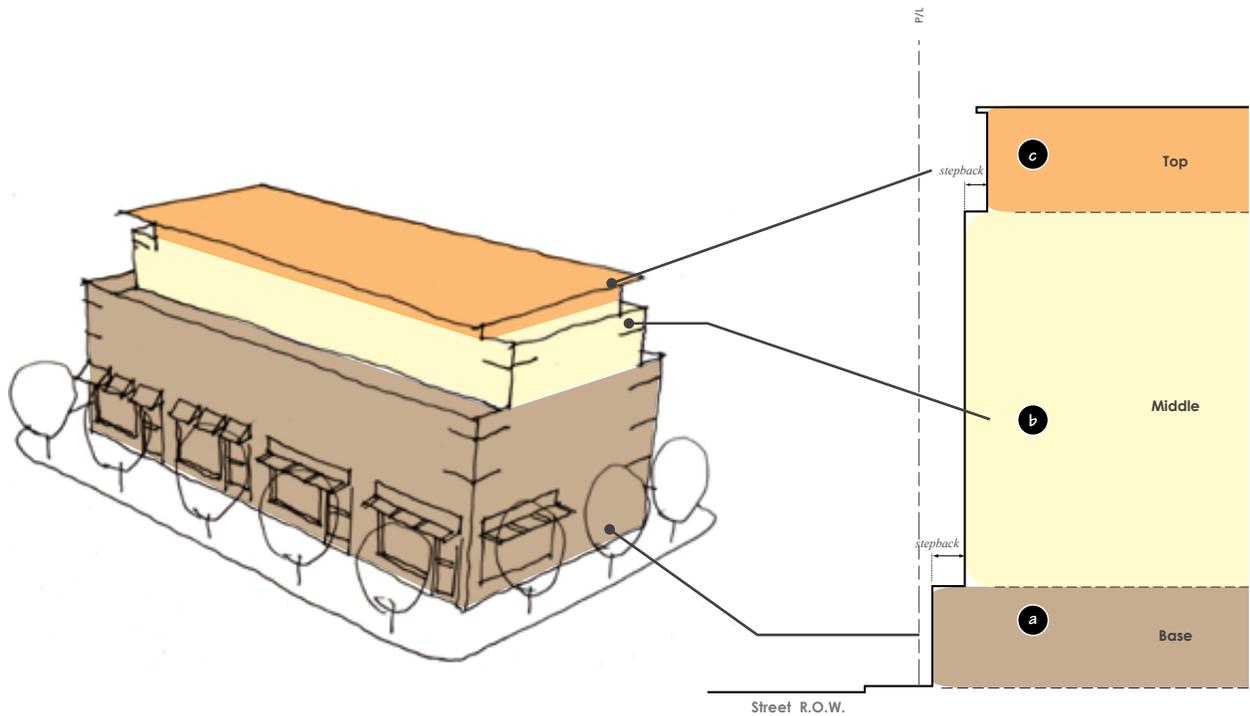
4.11.1 Building Expression

To encourage continuity in the streetscape and to ensure horizontal ‘breaks’ in the façade, buildings should be designed to reinforce the following key elements through the use of setbacks, extrusions, textures and materials:

- a Base** - Within the first three-storeys a base should be clearly defined that positively contributes to the quality of the pedestrian environment in the level of animation, transparency, articulation and material quality.
 - b Middle** – The middle or body of the building should contribute to the physical and visual quality of the overall streetscape.
 - c Top** – The roof condition, expressed as an upper storey or roof feature, should be distinguished from the rest of the building and designed to contribute to the visual quality of the streetscape.
- Buildings should seek to contribute to the mix and variety of high quality architecture. The articulation of building mass through vertical and horizontal recesses or projections, datum lines, and changes in materials, texture or colour should be encouraged.



Buildings should have a clearly articulated base, middle, and top





4.11.2 Orientation & Placement

The orientation and placement of buildings along the street help to reinforce the public realm by enhancing the pedestrian environment through creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings, providing the appropriate levels of animation and use. Key guidelines for the orientation and placement of buildings are as follows:



a All buildings should orient to and address the street with clearly defined entry points that directly access the sidewalk.

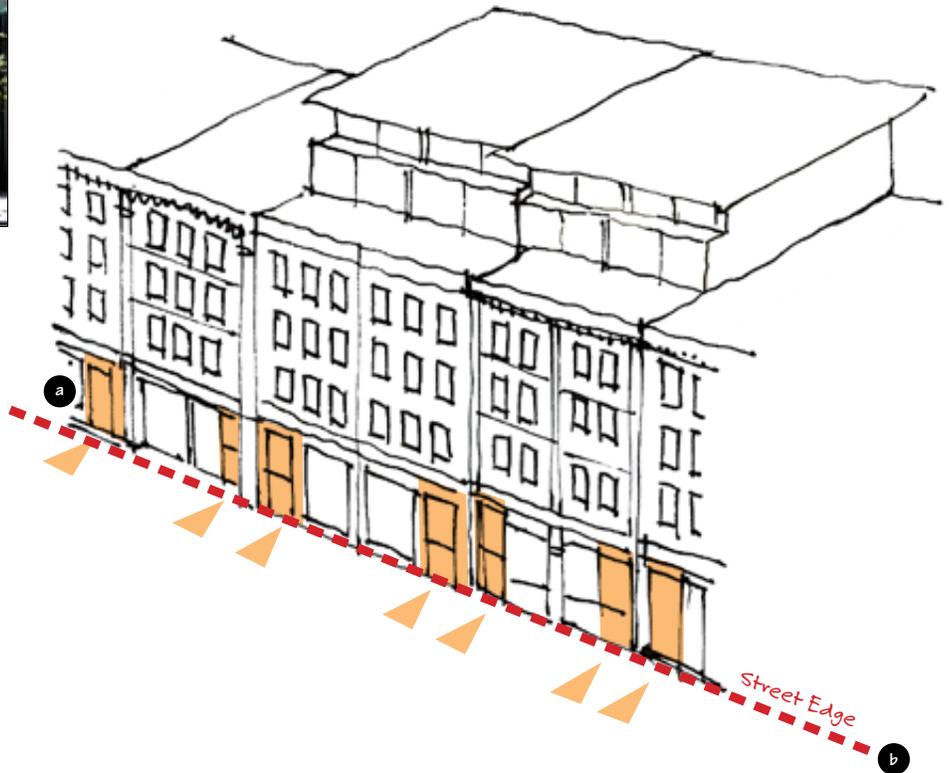
b Buildings should be placed at or close to the street edge, subject to the specified build-within zone.

- Development of an entire block or at corner sites may provide greater setbacks to widen sidewalks without compromising the visual continuity of the streetscape.

- To discourage fragmentation of the street wall and to encourage full utility of the rear lanes, 100% building coverage of the front-yard is encouraged and should be required for buildings on Broadway Avenue.



c Entrances to buildings should address the primary street and should be clearly articulated and expressed.

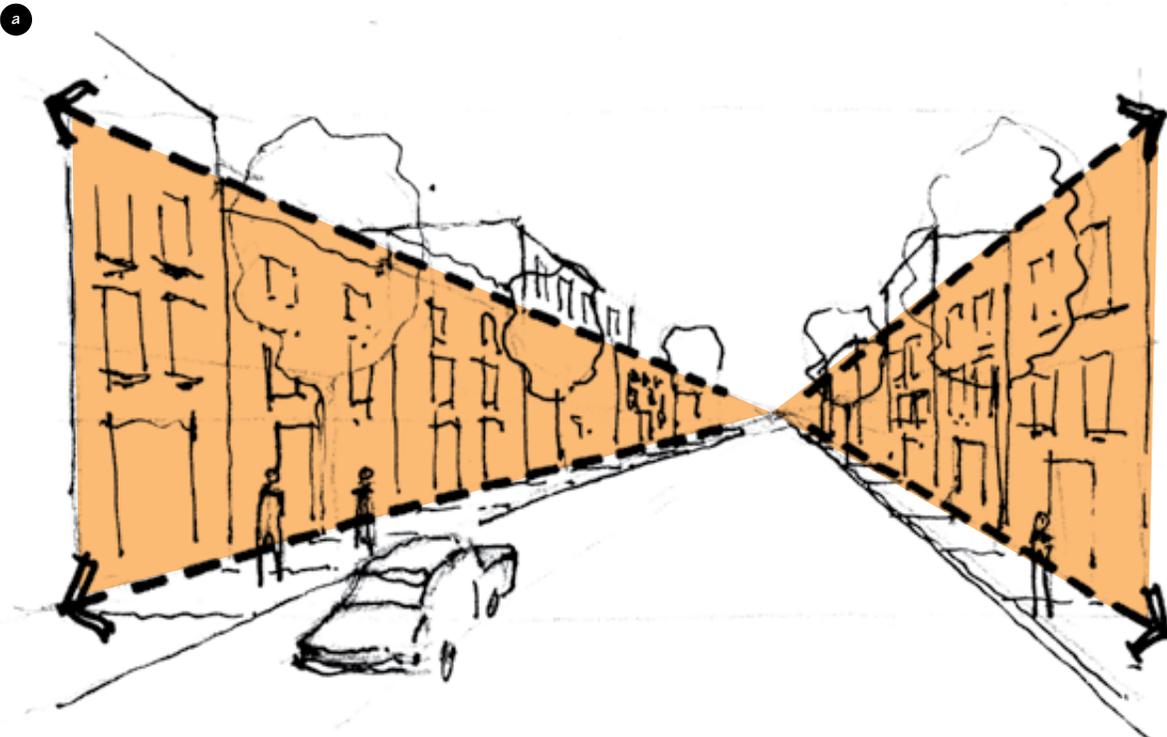




4.11.3 Street Wall

The street wall is the part of the building base that frames the street and interfaces with the sidewalk. The street wall has the greatest impact on the character and quality of the street experience. The key design objectives for street walls in the Broadway Area are ensuring visual continuity, pedestrian scale, animation of the street and design quality. Guidelines for street walls include:

- a** In general, a street wall of a new building should align with those of neighbouring buildings or have the same setback as the predominant buildings on the block.
- b** The height of the street wall should be consistent with historic heights of no greater than 3 storeys and no less than 2-storeys.
 - Levels above the street wall should be setback to reinforce a low-rise interface with the sidewalk.
- c** Grade-level heights should be visually prominent and no less than 4.5metres for commercial and 4.0metres for residential uses.
 - At-grade retail uses should be consistent with the design guidelines for storefronts.
 - Other commercial at-grade uses should have entries onto the street and include high levels of transparency.
 - At-grade residential uses should include units that directly access the sidewalk and consistent with the design guidelines for street-access units.
 - Upper levels of the street wall should be well articulated and include expressed window openings.





4.11.4 Heritage Contexts

Where a new building is proposed along Broadway Avenue adjacent to a heritage significant building, its design should be sensitive and complementary. These guidelines help to ensure the fit for new buildings so that they contribute, rather than detract from the distinct character of heritage significant properties.



General Guidelines:

- New buildings should avoid historical misrepresentation. Buildings tell the story of historical development of the area. It is important that the historical record does not get confused through the mimicry of past architectural styles.
- New buildings should be designed so that they do not appear to have been constructed earlier than they were.
- New buildings should consider and respect the scale, material and massing of adjacent heritage significant buildings.



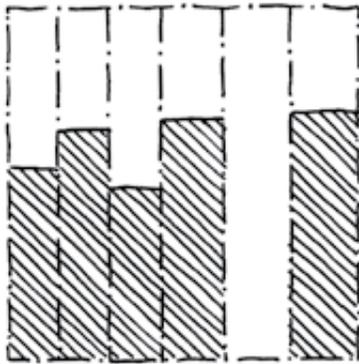
Façade articulation and materials should complement existing heritage, adjacent heritage buildings

Façade Articulation

- New buildings should respect the pattern of façade division by ensuring the horizontal and vertical architectural orders are aligned with neighbouring heritage buildings.
- New buildings should have entries and display windows at regular intervals consistent with the established pattern on the block.
- Windows should be vertically aligned from floor-to-floor and horizontally aligned with the neighbouring heritage buildings.
- New buildings should include a cornice that is carefully aligned with neighbouring heritage buildings and of similar proportions.

Façade Materials

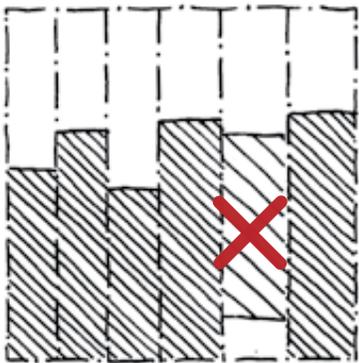
- New buildings should consider the pallet of materials and colours evident in existing heritage significant properties.
- Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, sustainability and ease of maintenance. Materials should also be chosen for permanence. Vinyl siding, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.



A potential site for in-fill



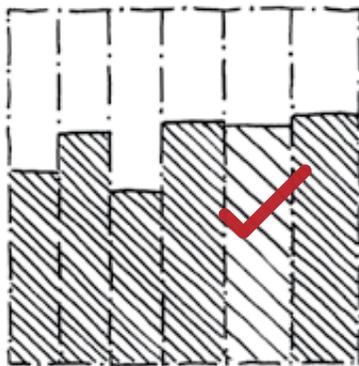
A potential site for in-fill



Improperly sited development, stepped back from the street line



Consistent building and ground storey heights



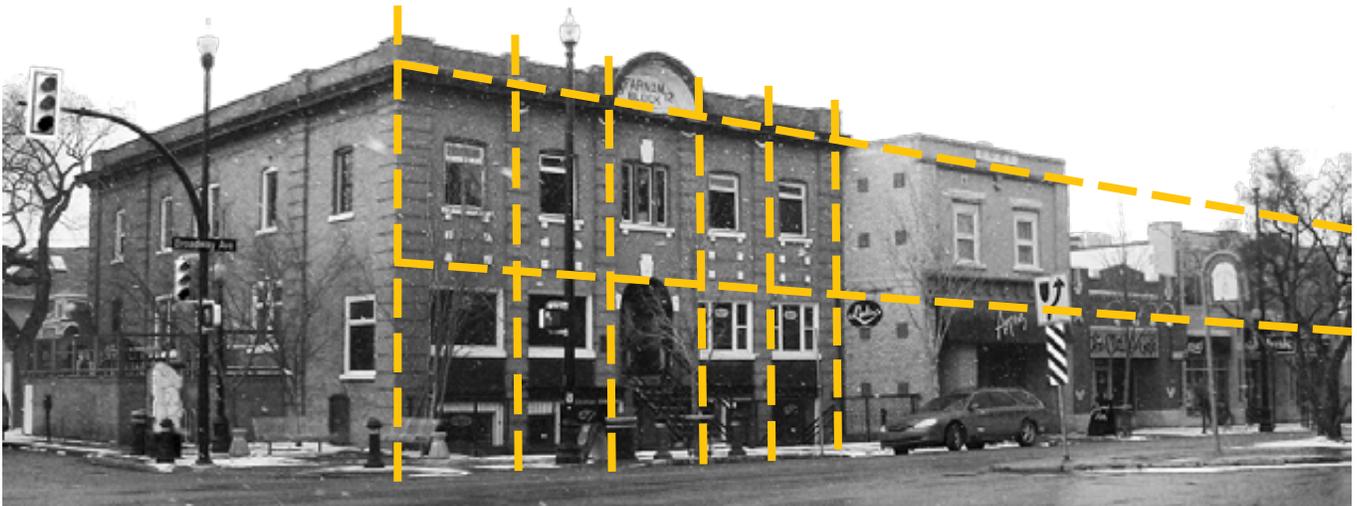
Properly sited development, built to the street line



Consistent horizontal alignment of façade elements



Consistent storefront and window placement with appropriately proportioned window openings



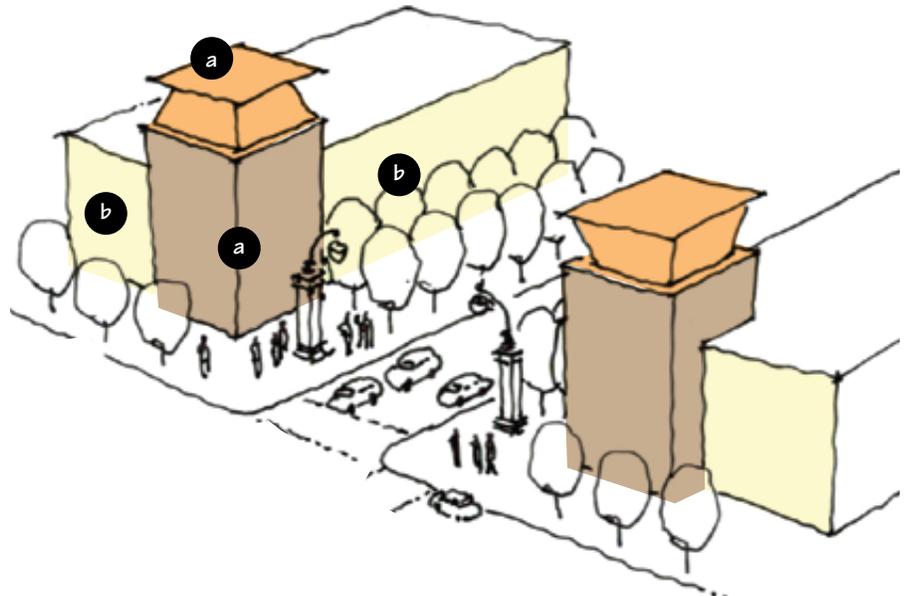


Examples of buildings with design responses to their location on corner sites

4.11.5 Corner Sites

Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections. Accordingly, they have a greater civic obligation to should be designed to give good form and address to the corners they occupy.

- a** To enhance the distinction of new buildings at Key Corner Sites, modest exceptions to setbacks and height restrictions should be permitted to encourage massing and designs that accentuate the visual prominence of the site – architectural treatments can include tall slender elements such as spires and turrets.
- b** New developments on all corner sites should orient to both street frontages.
- c** Corner entrances should be encouraged wherever possible, to give address to the two street frontages.
 - As new developments on corner sites can shape the image and character of an area, the highest possible standards in design and material quality should be encouraged.

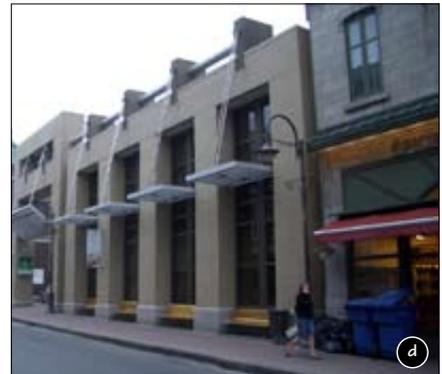




4.11.6 Storefronts

Well proportioned and designed storefronts can contribute positively to the pedestrian environment by providing animation and visual interest at the sidewalk.

- a** To reflect the existing character and context, storefronts should generally have a frontage in the range of 7.5 metres but not greater than 15 metres.
- b** Where retail frontages are greater than 7.5 metres, they should articulate narrow storefronts in the design of the facade.
- c** Storefronts should have a high-level of transparency, with a minimum of 75% glazing to maximize visual animation.
 - Clear glass should be used for wall openings (e.g., windows and doors) along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts.
 - An identifiable break or gap could be provided between the street-level uses and the upper floors of a building. This break or gap may consist of a change in material, change in fenestration, or similar means. The identifiable gap or break can emphasize the storefront while adding visual interest and variety to the streetscape.
 - Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade. Split level, raised or sunken entrances are strongly discouraged.
 - Storefront signage should be consistent with the signage guidelines, but generally should add diversity and interest to the street and not overwhelm either the storefront or the streetscape.
- d** Weather protection for pedestrians is encouraged through the use of awnings and canopies.
 - Storefronts that address the Mews should be permitted to have a greater freedom of expression in their design and treatment.



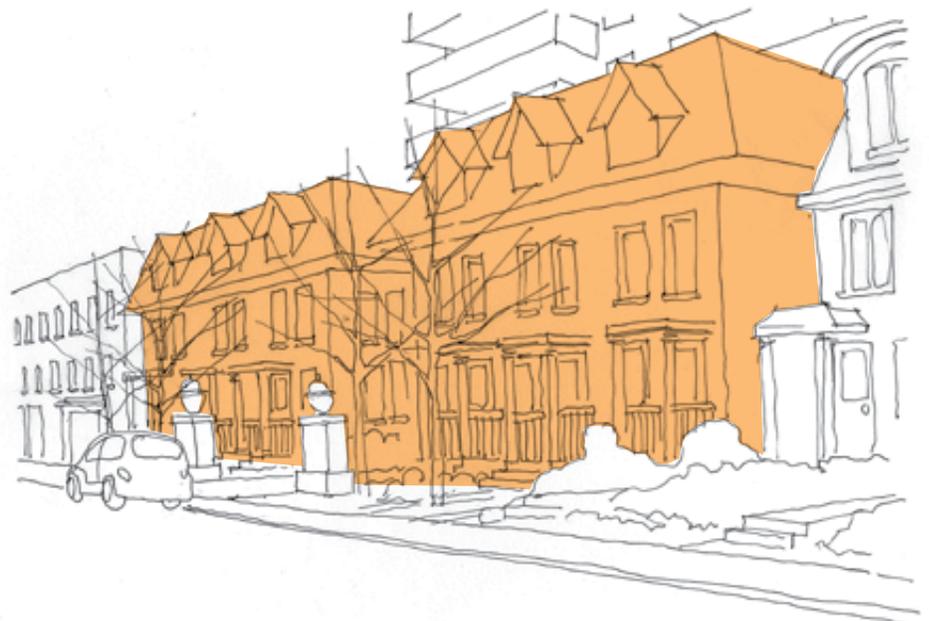


Examples of residential uses with street access

4.11.7 Street-Access Units

All uses should help create an animated street environment with doors, windows and pedestrian activity fronting and accessing directly onto the sidewalk. Where retail is not required, and residential uses are proposed at-grade, the following guidelines apply:

- Residential uses at-grade should include individual units accessed from the street.
- Appropriate front yard privacy measures should be considered such as setbacks, landscaping, and porches.
- Access to the individual units should be clearly visible, and the scale, rhythm and articulation of the street wall should be consistent with the residential character of adjacent neighbourhoods.
- Grade-level units should be designed to accommodate live-work opportunities and potential conversion into commercial or retail uses to complement the mixed-use context.





4.11.8 Roof Treatment

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs of buildings are also seen from other buildings of equal or greater height. Roof design should consider the following guidelines:

- a The expression of the building top and roof, should be clearly distinguished from the rest of the building through treatments such as stepbacks, change in materials, cornices lines, and overhangs.
- b Mechanical penthouses should be integrated with the architectural treatment of roofs and/or screened from view.
- c Green roofs should be encouraged to provide for aesthetic as well as functional and sustainable considerations.





Top and Right: Examples of above-grade parking structures that are designed to complement their surroundings with ground related uses

4.11.9 Above-Grade Parking

Wherever possible, parking for new developments should be provided at the rear or below-grade and accessed off the rear lane. Where parking is provided above-grade within the base building and if structured parking is contemplated for the Broadway area in the future, the following guidelines address the design and quality of such structures.

- Direct access for parking from the street should be discouraged.
- Where an above-grade parking facility fronts on a street, the ground-level frontage should incorporate retail, public or other active uses.
- Above-grade parking structures should be designed in such a way that they reinforce the intended built character and blend into the streetscape through facade treatments that conceals the parking levels and gives the visual appearance of a multi-storey building articulated with ‘window’ openings.
- Above-grade parking structures should provide articulated bays in the façade to create a fine-grain storefront appearance.
- Above-grade parking structures should provide pedestrian amenities such as awnings, canopies, and sheltered entries.
- Above-grade parking structures should utilize high quality materials that are compatible with other mixed-use buildings
- Stairways, elevators and entries should be clearly visible, well lit and easily accessible.
- Signage and wayfinding should be integrated into the design of public parking structures. Integrating public art and the lighting of architectural features should also be considered. This will reinforce its unique identity, and aid visitors in finding them upon arrival.
- The impact of interior garage lighting on adjacent residential units should be minimized, while ensuring that safe and adequate lighting levels are maintained.

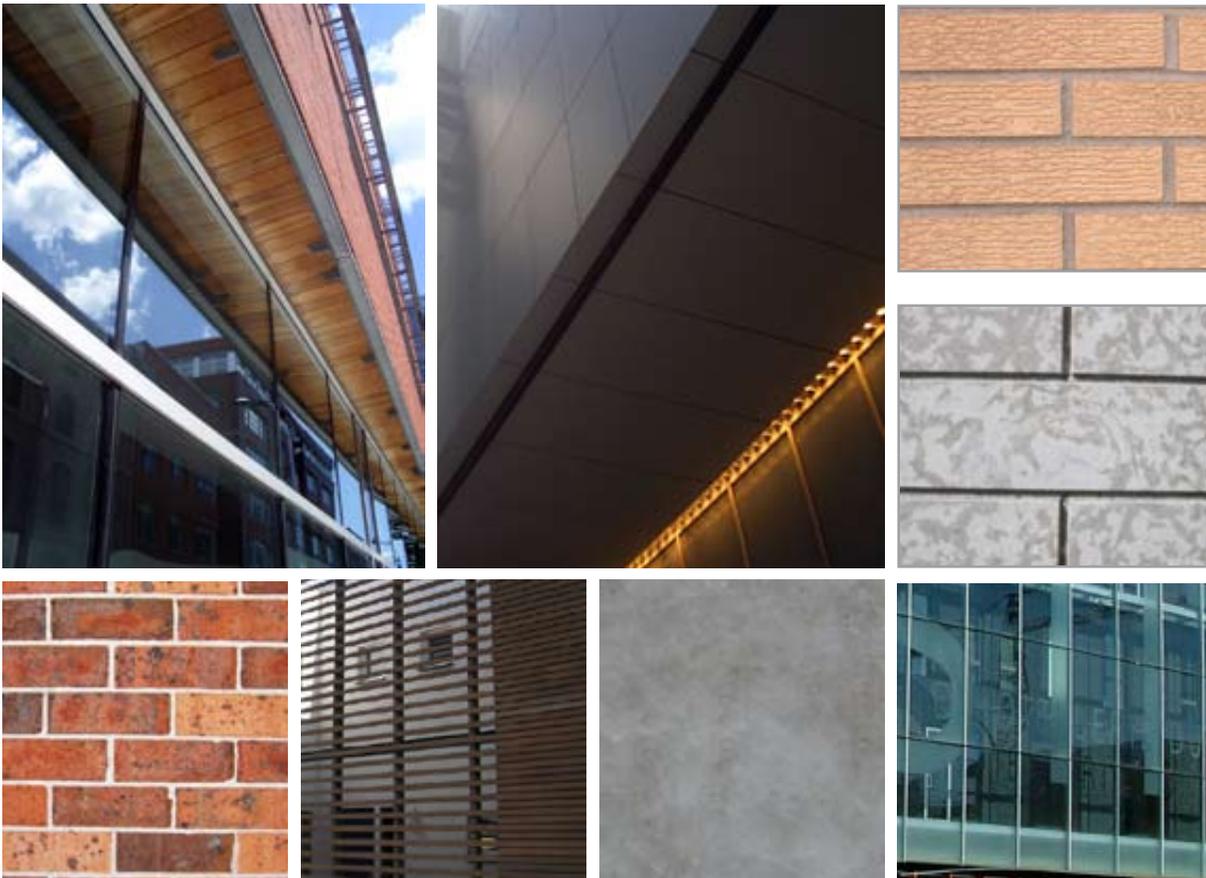




4.11.10 Material & Architectural Quality

New developments should be mindful of ensuring excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Development Plan is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- The Broadway area has a rich history of development that is reflected in the Prairie-style ‘main street’ buildings that are constructed in a variety of materials. New developments should seek to contribute to this mix and variety.
- Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete.
- In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- Vinyl siding, plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.





4.11.11 Sidewalk Cafés

Sidewalk cafés enhance the vibrancy of street life and are major destinations in the warmer months. Sidewalk cafés also serve as neighbourhood amenities enabling further social interaction on Broadway Avenue.



- Sidewalk cafés should be encouraged throughout the Broadway Area provided that there are no conflicts with adjacent land uses and that they are able to be accommodated within the existing sidewalk width dimensions without encumbering pedestrians.

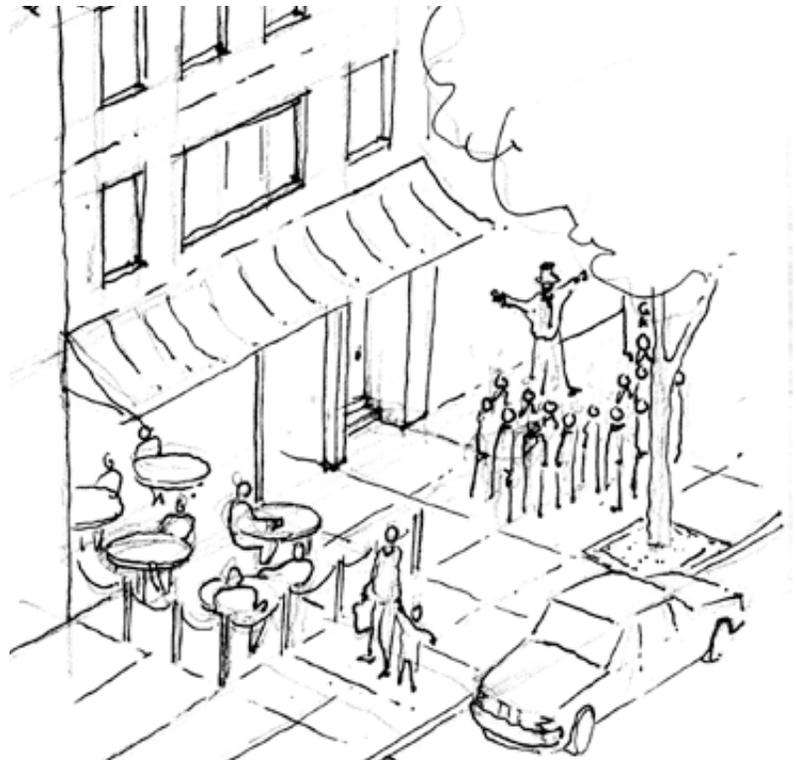
a Where permitting, small sidewalk cafés should be encouraged along streets with narrower sidewalks as well. Small sidewalk cafés generally require 1.4 metres for a single row of tables and chairs. This will comply with the *City of Saskatoon Sidewalk Café Guidelines (1990)* for maintaining a clear passage of 2.0 metres between the sidewalk café and the curb or any other physical obstructions.



- Sidewalk cafés should be designed to contribute and integrate into the streetscape. Tall fencing or landscaping that obscures visibility to and from the street should be avoided. Material and landscaping choices should be of the highest possible quality.

b Curb bump-outs should be encouraged at all corners to provide for additional sidewalk café opportunities.

- Rear yard and roof top patios should be directed to properties that are not directly adjacent to residential neighbourhood.





4.11.12 Building Lighting

The image and experience at night is an important aspect of any mixed-use area. Illumination of buildings through creative approaches to lighting has the potential to transform the image of an area and reinforce its identity and appeal. Considerations include:

- Attractive landscape and architectural features can be highlighted with spot-lighting or general lighting placement.
- Heritage and institutional buildings, as well as landmark elements such as public art, steeples or distinctive rooflines, should be illuminated.
- Subtle night-lighting of retail display windows should be encouraged.
- Ensure feature lighting does not spill onto adjacent residential areas and does not cause glare or other safety related issues.
- Ensure that lighting is consistent with the City of Saskatoon Zoning By-law 7800 and is Dark Sky compliant.





Projecting Signage



Banner Sign



Integrated Signage

4.11.13 Signage

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape. They should reflect the unique characteristic of their context. This includes compatibility with heritage buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged. Design objectives for commercial storefront signage include:

- All signage should conform to the City of Saskatoon Bylaw 7800 regarding signage group no.5.
- Commercial signage should not overwhelm the building and/or the storefront.
- Back lit illuminated rectangular sign boxes are discouraged.
- To minimize visual clutter, signage should be integrated into the design of building façades wherever possible, through placement within architectural bays and friezes.
- Signage should not obscure windows, cornices or other architectural elements.
- Large freestanding signs (such as pylons), roof signs, and large-scale advertising (such as billboards) should be discouraged.
- The maximum signage area for storefront signs should be no more than 25% of the business storefront.
- Signage on heritage buildings should be consistent with traditional sign placement such as on a sign band, window lettering, or within the existing architectural orders.
- Signage should aid pedestrians and drivers in navigating the area, especially at night.
- Signs should be well maintained and constructed using high quality materials.



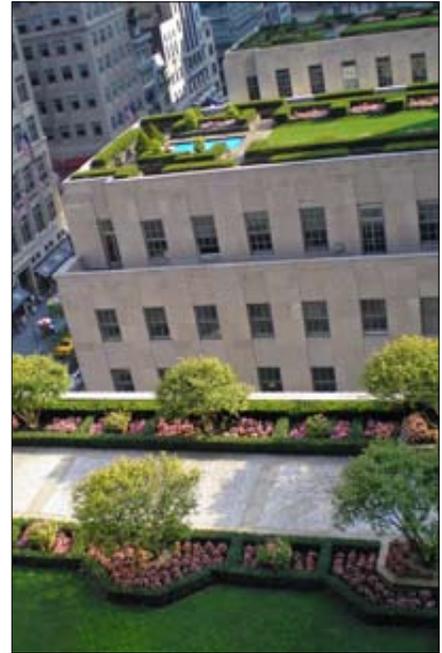


4.11.14 Sustainable Design

Sustainable design can be defined as architecture and engineering that establishes the conservation of natural resources and systems as a primary consideration in the planning, design, and construction process. To achieve this goal, all proposed projects should strive for sustainable building practices. This includes public as well as private development, and encompasses streets, parks, and buildings.

The City of Saskatoon, Broadway BID and the Nutana Community Association should urge LEED-certified levels of sustainable design and encourage the private sector to meet that challenge. In line with the sustainable strategies and LEED, opportunities exist to rehabilitate underused or deteriorating historic resources with new functions through adaptive reuse to strengthen the unique character of the area. As a principle of sustainability, new additions, exterior alterations, or related new construction should not destroy historic materials, features, and spatial relationships that characterize the property. The new work should be differentiated from the old and should be compatible with the historic materials, features, size, scale, height, proportion and massing to protect the integrity of the property and its environment.

A typical sustainable design standard to pursue is a LEED CaGBC (Canada Green Building Council) certified, Silver, Gold or Platinum. This requires all buildings to achieve at least 50% of the available LEED credits for sustainable design. More information on this program is available at the Canada Green Building Council's web site at <http://www.cagbc.org/>



Roof gardens that collect storm water and provide amenity spaces



Building with recycled and salvaged materials



Tenant recycling program



Dedicated bike storage



Surface runoff from the street is captured in a naturalized swale



Street lighting powered by the sun

5.0

implementation strategies

5.1 introduction

The Implementation Strategies provides recommendations for putting the Broadway 360° Development Plan into action. It outlines what the Broadway Business Improvement District, Nutana Community Association and City of Saskatoon could do to implement the various elements of the Plan. Specifically, this section outline recommended projects, programs, policies or strategies organized according to the three key theme areas that framed the terms of reference for the undertaking of the Broadway 360° Development Plan. As an integrated plan, these themes and their accompanying recommendations overlap in their influence on the long-term objectives for the area. They include:

- Land Use
- Atmosphere & Character Retention
- Transportation & Parking

The Implementation Strategies will be most useful in defining programs, setting priorities, allocating finances and assessing achievements. It is not expected or necessary that all these recommendations are carried out, rather this section offers a toolbox of potential actions that may be considered. Over time, this part of the Broadway 360° Development Plan could be revisited and updated to ensure that the strategies remain relevant and current as the Broadway area evolves.



5.2 landuse

Adopt the Recommended Development Standards

The Development Framework provides alternate development standards for the Broadway area, taking into consideration a combination of appropriate measures to ensure good form, relationships to streets and ensuring transition to adjacent low-rise neighbourhoods. Building on existing zoning standards, recommendations for alternate or additional standards for scale, height, massing, setbacks and density are provided where applicable.

The intent is to provide appropriate parameters and supporting rationale where necessary to inform and guide the City in preparing the enabling policies and mechanisms that will most effectively implement these standards. Rather than a complete rezoning, the Nutana Local Area Plan suggests an overlay zone that introduces a tiered approach tied to the scale of development. The Development Framework has been designed to complement a tiered overlay zoning approach and would not preclude harmonization with existing standards or the introduction of additional standards.

Consider an Architectural Control District

As a complement to the Development Framework, the Design Guidelines provide direction for the quality of design for a variety of built form elements. With recent changes to the Planning and Development Act, the municipality has been given greater powers to control the design and quality of new developments. As a means for ensuring that new buildings reinforce and enhance the best qualities of the Broadway area, an Architectural Control District can be established to give more weight to adhering to the Design Guidelines.

Given that a certain level of design license can ensure a variety of architectural responses to achieving common urban design objectives, the Design Guidelines are not intended to be prescriptive and rigid in their interpretation. They provide for flexibility and creative responses where appropriate, making them well suited if an Architectural Control District is to be considered.



5.3 atmosphere & character retention

Implement Public Realm Improvements

The Development Plan makes a number of recommendations with respect to public realm improvements that serve to enhance the atmosphere and character of the Broadway area. These improvements also serve to reinforce or strengthen desired land use decisions and to direct revitalization where it is needed. Chief among them are extending the Broadway Avenue streetscape vocabulary into all mixed-use areas, the Five Corners Plaza, the Mews, and improvements to the Oskayak School frontages.

The BBID in collaboration with the City and other applicable agencies should work towards prioritizing, funding, designing and implementing these recommendations over the long-term. Priorities should also respond to emerging opportunities such as new developments or capital works to coordinate efforts and share funding.

Identify and Register Heritage Resources

Given the historic importance of the area to Saskatoon, every effort should be made to retain, restore, fund and promote Broadway area heritage. A number of buildings along Broadway Avenue are listed but not designated and there may be other properties of interest, including those identified in this Plan, which may be of heritage significance. The City should be proactive in undertaking an inventory and assessment of the heritage resources in the area to ensure that appropriate protective actions are taken in advance of any potential threats that may emerge with ongoing infill and development.

Prepare Neighbourhood Infill and Architectural Guidelines

Although the greatest change and growth is anticipated for the Broadway Avenue corridor for which this Development Plan serves to guide in significant detail. The surrounding historic residential neighbourhood is also subject to changes and infill development, which can undermine their quality and character if not carefully considered. Design guidelines should be prepared that are specifically tailored to Nutana's residential neighbourhoods. They could provide guidance to landowners and developers on appropriate infill, additions and renovations, while assisting City staff in reviewing and assessing applications.

Coordinate Approaches to Addressing Incivilities

Through the course of the planning process, issues relating to licensed establishments were reviewed and considered. The primary concern focused on noise and the incivilities of patrons that frequented local bars and nightclubs. The City has already implemented commonly used tools and measures to deal with these issues, including:

- Limitation on the number of licences
- Nuisance By-Law
- Enhanced police presence

Moving forward there are a number of potential strategies that should be considered for preventative measures and effective enforcement:

- Coordinated enforcement – The Police and Fire Departments and the Saskatchewan Liquor and Gaming Association (SLGA) should co-ordinate efforts so when infractions are issued all departments should issue them at once to revoke a license.
- Issue fines not charges – pressing charges requires police officers to return to the station, removing them from the very context that needs their constant presence and surveillance.
- Conditions for licensing – clear conditions should be established for issuing licenses.

Undertake an Awareness Campaign on the Community Benefits of Festivals

There are broad social, economic and profile benefits to successful festivals and events in communities. The Broadway area has enviable success in its Fringe Festival and Street Fair, which draw people from across the city and beyond. These events enhance the profile of the area and are an important aspect of what makes this neighbourhood so appealing and desirable.

The benefits of these events to the community should be promoted through an awareness campaign that serves to provide meaningful information but also to generate interest and involvement from the broader community in their execution. In doing so, the entire community can take ownership in these events and have a vested interest in their continued success. At the same time, matters of street closures, scheduling and other logistical matters can become a shared decision and responsibility.

implementation



5.4 transportation & parking

Consider Traffic-Calming Measures to Improve Pedestrian Safety

Through the course of the process it became clear that there were concerns around the speed and infiltration of traffic and the associated risks for pedestrians. Relative to other comparable urban areas, there are minimal traffic-calming measures that have been implemented in the Broadway area. On-street parking plays an important role in slowing down traffic and enhancing pedestrian safety. Although this study has determined that there are currently no significant traffic issues in the Broadway area, there are a number of recommendations that would lend to enhancing pedestrian safety. This is especially important for a transit supportive, pedestrian-oriented retail and high-density environment

- The timing for pedestrian crossing at green lights on east-west streets should be increased. Currently they do not provide enough time for pedestrians to comfortably cross within the timeframe given. Increasing the timing will not only make it safer for pedestrians, but it will also convey the message that pedestrians are important in this area.
- Existing signaled intersections should be fitted with a pedestrian countdown signal to enable walkers to better negotiate their timing for crossing the street.
- The City of Saskatoon should explore the installation of signalized crosswalks and/or bump-outs on streets near Broadway area schools to help improve safety for students and residents alike, provide additional mid-block connections, and help calm traffic speeds.
- Stop signs accompanied by clearly marked crosswalks should be introduced at key, if not all, four-way intersections within the residential areas. This will also help to slow down traffic and discourage motorists from using these streets as through-routes.
- Although the feature pavement treatments and the introduction of other elements in the Mews will serve to slow traffic speeds, other measures such as speed bumps should be considered in other rear lanes to discourage through-traffic and speeds.

Explore Potential Routes for On-Street Bicycle Lanes

A key strategy to improving parking and traffic concerns will be to encourage active transportation choices such as walking, transit and cycling. A comprehensive study should be undertaken to explore and identify potential on-street bicycle lanes on appropriate streets in the Broadway area, including the potential conversion of the Victoria Avenue Bridge into a pedestrian/cycling only route. Although, the Broadway Avenue Bridge may also be considered as a crossing point for cyclists, a dedicated lane within the commercial area should only be considered through travel lane narrowing and not at the expense of on-street parking or sidewalks. As with all modes of movement, Broadway Avenue should be considered as a destination for cyclists with adequate parking provided.

Better Utilize the Parking Supply

According to the 2000 Parking Study, there are approximately 760 on-street parking stalls within the area bounded by Eastlake Avenue, 8th Street East, Dufferin Avenue, and 12th Street East. Based on this and other previous studies and analyses, the Broadway area does not have a parking shortage problem. The area has a significant on-street and off-street parking supply in the range of an estimated 1500 spaces.

The on-street parking supply is very visible and in the most heavily used locations the supply is controlled by parking meters. Signs, especially in front of the residential uses, control the majority of on-street parking. What is not visible is that:

- in the commercial areas in back of the stores there is parking available which is not utilized; and
- most homes have parking available off the laneways – also off-street parking that is not utilized.

Furthermore, the lack of parking delineation lines leads to inefficient use of the available on-street space.

Currently the Broadway area is providing free on-street parking that manages to meet the needs of not only the residents and shoppers, but also the broader community. On-street parking in some locations is taken up by long-term parkers who either attend Nutana Collegiate or take the bus to the University. Workers who walk to the Downtown also take up significant numbers of on-street parking spaces.

This demand will clearly re-locate once development and re-development occurs in the area. Therefore in the initial stages of development and re-development of the area, parking should not be the limiting factor to prevent development. Instead the priorities should be to:

- Force (long term) on-street parkers to leave the area so that the supply is used by businesses within the area.
- Identify and implement a shared parking strategy amongst all available off-street parking lots, including schools, such that the peak demands of each are accommodated in different hours of the day. Shared parking is already implemented at the Victoria School and could be extended to include Oskayak School, and businesses on both sides of Broadway Avenue.
- Improve signage and awareness programs to direct visitors to available parking areas, including available areas to the rear.
- Clearly demarcate potential parking stalls to ensure efficient use of available spaces on street – a solution will need to consider all season.
- Improve enforcement of parking violations.
- Encourage residents to park in their respective driveways via rear lane and the City should ensure adequate snow removal to do so.
- Implement a parking permit policy program for the residential area, especially where there is a proliferation of rental units. Renters should not compete for the same space that is serving the retail / commercial operations. Their parking demand needs should be solved through their participation in a permit program - even if that permit program has to identify remote parking areas to serve their needs.
- In the medium to long-term as development and re-development occurs, a supply of public parking can be incorporated within one or more parking structures built in association with developments.
- Encourage greater usage of public transit and cycling to and from the area.
- Continue to monitor the parking demand and supply to ensure that it is addressed when it truly becomes an adverse issue for the area.

Appendix **A**:

Stakeholder Consultation Outcomes

Appendix A: Stakeholder Consultation Outcomes

The following is a compilation of the key assets, issues and opportunities identified by stakeholders during the focus group sessions held over the course of two days, from **September 16 to September 17, 2009**. This summary is organized around three themes: **Key commonly shared perspectives; key contradictory perspectives; and, other key items** that were raised by participants.

The key commonly shared perspectives are those issues and opportunities identified by at least two or more groups during the focus group sessions. The key contradictory perspectives are those that were identified as contradictory by different groups. Lastly, a brief overview of some other key points raised by groups or individuals during the sessions that are important to highlight for the Broadway 360° Area Development Plan have been included.

Key Commonly Shared Perspectives

Commonly shared perspectives among the majority of stakeholders interviewed include the following:

Assets of the Broadway Area:

- The central location of Broadway Avenue and the surrounding area has an enviable location within the City of Saskatoon. The area has a **central location** that is easily accessed from all parts of the City.
- The Broadway Area has the distinction of being the original settlement for the City of Saskatoon and as such, many of the buildings each tell its own story. When read individually, the buildings showcase the significant built **heritage** of the area. When read together, the buildings and the landscape tell the story of the development of the City.
- the Broadway Area has a diverse **mix of uses** that contributes to the character of the neighbourhood.
- Residents and visitors alike appreciate the **diversity – socially, economically, demographically** – of the neighbourhood. Many have said that there is no other community in Saskatoon with such a wide range of the different people coming together.
- **Locally Owned Businesses** - Many of the owners/operators along Broadway Ave are also residents in the surrounding neighbourhood. This contributes to the close, intimate neighbourhood feel and is a distinguishing characteristic of the area that separates it from the rest of Saskatoon.
- The Broadway Area is considered **one of the densest parts of the city**, with a larger population adding to the vitality and energy of the street that sets it apart from other commercial retail streets in the City.
- The Broadway Area is one of the most **Walkable** parts of the City. With the dependency on the automobile in Saskatoon, the ability to walk or bike to meet your daily needs is highly regarded.
- Residents and business owners all considered the **festivals and events** to be an asset for the area.
- The Broadway Area has a **strong sense of community**.
- The **Business Improvement District (BID)** is considered a key asset to the area, and is one of the main drivers of the area's success – such as the streetscaping, which many residents feel contributes to the character and identity of the area.

Issues:

- All the stakeholders we met with identified **parking** as an issue. Residents are concerned with the infiltration of parking into their neighbourhood, while business owners are concerned that there may not be enough parking spaces available. The business owners did acknowledge that there is increasing acceptance by customers that they will not be able to park directly in front where they want to go and may have to walk a bit.
- The Broadway Area has tremendous heritage value and is an important part of the history of the City of Saskatoon, but there is concern that the policies and tools surrounding **heritage management** are insufficient.
- Numerous stakeholders raised increasing **Development** pressure and a fear that new developments will threaten the character of the Broadway Area as a concern.
- **Affordability** for both residential and commercial uses is increasing and may threaten the identity of the area. As the Broadway Area increases in value, the locally owned business and diversity of residents may be driven out as they may no longer be able to afford to remain.
- **Threat from Other Emerging Areas** primarily new developments on the other side of the Saskatchewan River along the riverfront and in Downtown Saskatoon. There were concerns expressed that if the Broadway Area did not remain proactive in dealing with the issues, that the other emerging areas would surpass Broadway and the area would fall into decline once again.
- Concerns were raised around the **noise, traffic and litter** related to some of the activities associated with the bars and pubs.

Opportunities:

- Many of the stakeholder groups identified **LEED and environmental sustainability** as opportunities for new developments in the Broadway Area. Many expressed a desire for the Broadway Area to emerge as a leader in environmental design and sustainability, ensuring that new developments met or exceeded LEED standards.
- While there was not consensus regarding the general safety of the Broadway Area, many of the stakeholders interviewed mentioned improvements to the physical environment to prevent the opportunity for crimes to occur. Opportunities exist to implement general **Crime Prevention through Environmental Design** principles such as improving the lighting in the rear lanes, and cutting or trimming back some of the low lying shrubs and plants.
- In all the groups interviewed, opportunities to **improve alternative modes of transportation** were raised. This includes better transit service and improving infrastructure for bicycles and other forms of 'self-propelled' transit. It was also raised that connections for all forms of transportation could be improved.
- To address the issue of affordability, it was raised by more than one group that existing policies need to be amended so innovative ways of providing **affordable housing**, such as **granny flats** about garages in the rear lanes, are permitted.
- **Density** was raised as an opportunity moving forward for ensuring that the retail, restaurants and services along Broadway Avenue remain financially viable.
- There are opportunities for introducing new incentives and tools for **heritage protection and enhancement**.
- Limiting or specifying **store size** (floorplate sizes) was suggested as an opportunity for ensuring that the locally owned shops and restaurants remain a part of the character and identity of Broadway.

Key Contradictory Perspectives:

There were only two points raised among the various stakeholders that were contradictions, safety and gentrification. While many of the stakeholders expressed that they generally felt safe in the Broadway area, many others mentioned that they do not. Personal **safety** was something where there was no consensus.

Gentrification was seen as both positive and negative by some of the stakeholder met with. Some stakeholders felt that gentrification in the Broadway Area was a positive contribution to an area that has only recently come into its own. Others felt that the gentrification of the area was the primary cause to many of the issues that the neighbourhood is facing such as increasing parking issues, developments that were sent to threaten the area, and a loss in diversity of both residents and businesses.

Other Key Notable Perspectives:

- Many expressed that part of the character and identity of Broadway Avenue is that while it plays a citywide role, it has maintained a **small town character anchored by the “Main Street Experience.”**
- The **authenticity** of both the experience of being on Broadway, and the authenticity of the heritage buildings along the street was raised as one its distinguishing qualities. Many stakeholders expressed that they did not like façadism or mimicry, and that new buildings should be of its time.

Appendix **B**:

List of Stakeholder Groups

The following is a list of the stakeholder groups consulted during the Broadway 360° Summit, held September 16 and 17, 2009.

- Broadway Business Improvement District Merchants
- Commercial Landowners
- Oskayak High School
- Nutana Collegiate
- Victoria School
- Koimisis Tis Theotokou Greek Orthodox Church
- Saskatoon Heritage Society (SHS)
- Municipal Heritage Advisory Committee (MHAC)
- Nutana Community Association
- Broadway Initiative for Liveable Development (BILD)
- Area Residents
- City Staff

